

MINUTES

PLANNING COMMITTEE

8 DECEMBER 2015

APPROVED FOR RELEASE

A handwritten signature in black ink, appearing to read 'Martin Mileham', is centered within a rectangular box.

**MARTIN MILEHAM
ACTING CHIEF EXECUTIVE OFFICER**



CITY *of* PERTH


MINUTES

PLANNING COMMITTEE

8 DECEMBER 2015

THESE MINUTES ARE HEREBY CERTIFIED AS
CONFIRMED

PRESIDING MEMBER'S
SIGNATURE


DATE: 27/1/16

PLANNING COMMITTEE

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Minutes of the meeting of the City of Perth **Planning Committee** held in Committee Room 1, Ninth Floor, Council House, 27 St Georges Terrace, Perth on **Tuesday, 8 December 2015**.

MEMBERS IN ATTENDANCE

Cr McEvoy – Presiding Member
Cr Adamos
Cr Yong

OFFICERS

Mr Stevenson – Chief Executive Officer
Mr Mileham – Director City Planning and Development
Mr Ridgwell – Manager Governance
Ms Smith – Manager Development Approvals
Mr McDougall – Interim Manager Economic Development
Mr Smith – City Architect
Mr Fotev – 3D Model Coordinator
Mr Close – Economic Development Officer
Ms Emmons – Governance Officer

GUESTS AND DEPUTATIONS

Nil

PL223/15 DECLARATION OF OPENING

5.33pm The Presiding Member declared the meeting open.

PL224/15 APOLOGIES AND MEMBERS ON LEAVE OF ABSENCE

Nil

PL225/15 QUESTION TIME FOR THE PUBLIC

Nil

PL226/15 CONFIRMATION OF MINUTES

Moved by Cr Adamos, seconded by Cr Yong

That the minutes of the meeting of the Planning Committee held on 10 November 2015 be confirmed as a true and correct record.

The motion was put and carried

The votes were recorded as follows:

For: Crs McEvoy, Adamos and Yong

Against: Nil

PL227/15 CORRESPONDENCE

Nil

PL228/15 DISCLOSURE OF MEMBERS' INTERESTS

Nil

PL229/15 MATTERS FOR WHICH THE MEETING MAY BE CLOSED

The Presiding Member advised that in accordance with Section 5.23(2) of the Local Government Act 1995, the meeting will be required to be closed to the public prior to discussion of the following:

| Confidential Item No. and Schedule No. | Item No. and Title | Reason |
|--|---|-------------------------|
| Confidential Schedule No. 4 | Item No. 1 – Hill Street Pump Station No. 2 – Community Consultation Results, Proposed Maintenance Works and Expression of Interest Process | Section 5.23(2)(e)(iii) |
| Confidential Schedule No. 7 | Item No. 2 – Heritage Grant Application For The Preparation of a Conservation Management Plan For Three Houses, 55-59 Goderich Street, East Perth | Section 5.23(2)(e)(iii) |

PL230/15 HILL STREET PUMP STATION NO. 2 – COMMUNITY CONSULTATION RESULTS, PROPOSED MAINTENANCE WORKS AND EXPRESSION OF INTEREST PROCESS

BACKGROUND:

| | |
|--------------------------|---|
| FILE REFERENCE: | P1027671 |
| REPORTING UNIT: | Economic Development |
| RESPONSIBLE DIRECTORATE: | Economic Development and Activation |
| DATE: | 27 October 2015 |
| MAP / SCHEDULE: | Schedule 1 - Boundary Rationalisation Schedule 2 - Summary of Submissions Schedule 3 - GHD Consultation Outcomes Report 2015 Confidential Schedule 4 - Colliers International Preliminary Commercial Assessment 2015 (Distributed to Elected Members under a separate cover) Schedule 5 - GHD Structural Condition Audit 2015 |

At its meeting held on **5 August 2014**, Council considered the Stage 1 consultation and approved the boundary realignment as the first step in the transition of the management order for the Langley Park Pump Station from the Water Corporation to the City of Perth. At that meeting, Council resolved that it:

- "1. notes the Department of Lands have agreed to expedite the reserve modifications via an extension to the Langley Park 'A Class' Reserve 12510, incorporating Reserve 13949, Lot 483 (Hill Street Water Pump Station No. 2),*
- 2. notes that the reserve will come under the City's care control and management, as part of a 'minor amendment' pursuant to section 42(3) of the Land Administration Act 1997;*
- 3. approves (Stage 2) consultation, seeking community comments / feedback, pursuant to the objectives of Council Policy 1.2 - Community Consultation, consisting of:*
 - 3.1 letters to adjacent landowners within 200m of the site (as shown in Schedule 8 [of that report]), being approximately 1,200 ratepayers;*
 - 3.2 letters to key sporting, events and recreation groups commonly associated with the reserve;*
 - 3.3 information and links to be placed on the City's webpage;*
 - 3.4 notices to be placed in the City of Perth office reception and library; and*
 - 3.5 a community forum informed by the submission received on the aforementioned advertising, to be formally hosted by a WALGA approved professional facilitator;*
- 4. notes that the results of the community consultation process will be reported to Council to inform an Expressions of Interest process;*

5. *notes the Water Corporation have agreed Expressions of Interest process cost sharing, including structural and feasibility reports."*

Pursuant to resolution 3 through 5 above, this report addresses: the Stage 2 consultation process; Water Corporation cost negotiations; and the forthcoming EOI and tender processes for Council's information / approval.

The Department of Lands (DoL) survey branch is currently liaising with the City's nominated surveyor GHD for the preparation of the Deposited Plan. Once the Deposited Plan is complete and in order for dealings, the DoL will prepare and lodge documents in order to finalise this proposal.

LEGISLATION / STRATEGIC PLAN / POLICY:

| | |
|---|--|
| Legislation | 42(4)(a) of the <i>Land Administration Act 1997</i> Section (13) of the <i>Planning and Development (Local Planning Schemes) Regulations 2015</i> <i>Heritage Act 1990</i> |
| Integrated Planning and Reporting Framework Implications | Corporate Business Plan Council Four Year Priorities: Perth as a Capital City S5 Increase place activation and use of under-utilised spaces |
| Policy | |
| Policy No and Name: | 1.2 – Community Consultation 9.15 – Contributed Asset |

DETAILS:

It is anticipated that the DoL reservation and Landgate title processes should be completed in approximately three months. As such, to ensure a timely outcome to the project, the City is looking to commence the Expressions of Interest process. The timing is also important in light of the increasing dilapidation of the State Heritage Listed building and the impact on the amenity of the locality.

Consultation

The proposed adaptive reuse was advertised from 10 to 24 August 2015, via:

- Letters to 1200 adjacent ratepayers;
- Letters key sporting, events and recreation groups commonly associated with the reserve;
- Information / notices on the City's web site, office and library.

There were a total of 13 submissions received, 12 from the public and one from an events company. The summary of the submissions are included in Schedule 2.

The adaptive reuse of the building and its curtilage was supported by all submissions with a focus on a café / dining use an overwhelming theme, incorporating some historical aspect of the building and Langley Park (i.e. aviation history).

Community Forum

A community forum was professionally facilitated by GHD, who have also been engaged to undertake the Structural Assessment, Asbestos Survey, and surveying for the boundary rationalisation.

The forum was held at the City of Perth offices on 10 September 2015, with six respondents and four attending. A summary of that session is included in the GHD Consultation Outcomes Report included in Schedule 3.

The key outcomes of this forum were:

- There is a clear community desire to see further activation of the Langley Park Reserve, and that the introduction of a new use to be housed in the existing pump station is strongly supported by the community.
- The workshop confirmed that the preferred future use for the Pump Station is for a café, noting the identified deficiency of café options away from the main CBD thoroughfare (Adelaide Terrace).
- The introduction of a café use is regarded as a compatible use that will complement and enhance the existing uses already occurring on Langley Park. It is highly probable that the café will be well attended, given the significant number of residential premises within the immediate locality.

Consultant Reports

The following reports have been prepared in support of the proposal, commissioned and funded in conjunction with the Water Corporation who currently retain the management order over the reserve:

- Colliers Preliminary Commercial Assessment – December 2011 (Updated 2015)
- GHD – Asbestos Survey September 2011 (remains valid – non asbestos found)
- GHD – Structure Assessment October 2011 (Updated 2015)

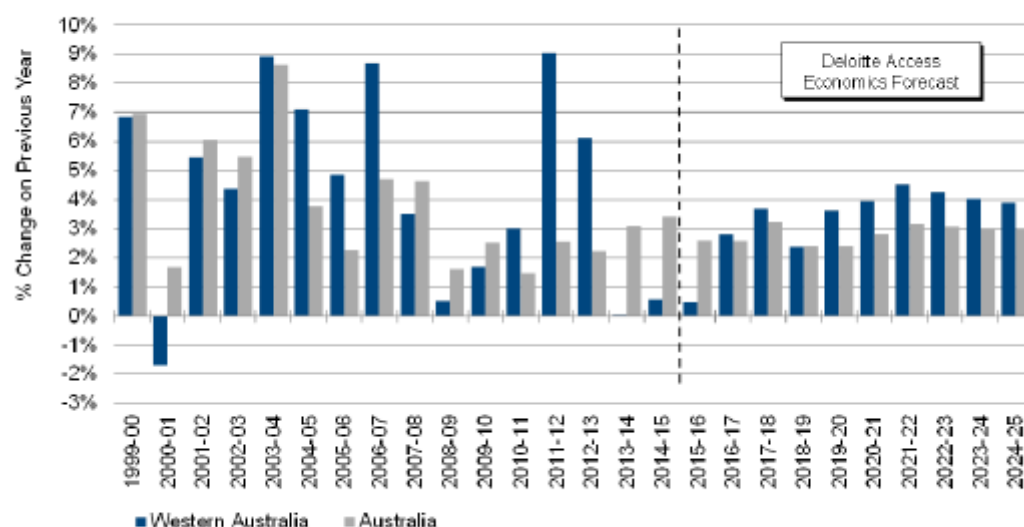
Colliers International – Preliminary Commercial Assessment (2015)

The 2011 Colliers International Preliminary Commercial Assessment has been updated to address current market conditions and is included under Schedule 4. The preliminary commercial assessment has been undertaken on an ‘as if complete’ basis, whereby all construction had been satisfactorily completed at the date of the report on the current market conditions. The Assessment made the following key observations:

- A small café / takeaway health food venue is recommended in light of the low foot traffic and leveraging the large number of casual sports participants.

- The location is commensurate to coffee destinations such as Yelo Café (Trigg), South Beach Café (South Fremantle), Lowquay Café (Wilson), Deep Water Point Café (Mount Pleasant) or similar.
- Given the relatively small internal size of 78sqm, it is recommended an alfresco be included.
- Limitations include disable access, toilet facilities, internal flooding of valve pit areas and low foot traffic in the area.
- The City of Perth to rectify the issues noted in the GHD Structural Assessment prior to offering the premises to market.
- Despite current economic conditions, the café / restaurant sector is still considered relatively stable, particularly in unique locations such as the subject site. Project retail turnover is positive is between 2.25% and 4.5% growth for Western Australia until 2024-25 (Refer Figure 1 below).

Real Retail Turnover Growth Forecast
(Constant Prices)



Source: Deloitte Access Economics Business Outlook - June quarter 2015 Colliers International

Figure 1 - Real Retail Turnover Growth Forecast (Deloitte, Colliers International)

- Rent incentives to attract tenants, including rent free periods could be offered.
- A rental rate range of approximately \$500 – \$600p/sqm per annum gross would be applicable for the internal space 'as if complete'. Alfresco areas could generate \$150 – \$250p/sqm. With an alfresco of 35sqm, Colliers expect the property could achieve circa \$44,000 - \$55,000 per annum gross 'as if complete'. This figure will vary significant dependent on the operator / development proposal for the adjacent curtilage.

While the report notes zoning limitations as a concern, this is not correct given that any use for the site could be considered as long as it was ancillary to and in support

of the broader Parks and Recreation Reserve under the Metropolitan Region Scheme.

GHD – Structure Condition Audit (2015)

As noted in Sections 4.1 and 4.2 of the GHD Structural Condition Audit (2015), (Schedule 5) urgent works have been identified as necessary to make the building safe for use and / or required to maintain the heritage fabric / integrity of the State Listed building (Ref. 03298).

Pursuant to Section (13)(2) of the *Planning and Development (Local Planning Schemes) Regulations 2015*, if the local government forms the view that a heritage place is not being properly maintained the local government may issue a written notice requiring specified repairs to the heritage place by a specified time (not less than 60 days). Furthermore, sub-sections (3) and (4) allow the City to undertake repairs outlined in the notice and recover expenses.

In this regard, the City is currently seeking a commitment from the Water Corporation to bring the building to a safe standard and addressing the ongoing dilapidation under its management and control. The City is prepared to offer a cost share 50/50 contribution to those works.

The City, DoL and the Water Corporation are yet to finalise consideration of this cost sharing arrangement and the transfer of the management order. As recommended in the Colliers International Preliminary Commercial Assessment 2015, the completion of these works is considered a priority for realising an appropriate rental yield in negotiations with prospective tenants. Furthermore, this approach ensures the heritage fabric of the building is not further undermined and the amenity of the locality is immediately improved.

FINANCIAL IMPLICATIONS:

There is no specific budget for the construction considerations for this project. It is envisaged the annual maintenance and life cycle matters will be negotiated with the future tenant following the EOI process under a favourable lease arrangement cognisant of these responsibilities. In this regard, assuming the City and the Water Corporation action the identified urgent and short term works, these ongoing costs are considered minimal within a typical long term lease cycle.

COMMENTS:

While there are several options around staging the improvements to the site, including negotiating those urgent heritage improvements with subsequent tenants, this is not recommended as:

- The Water Corporation and the City have legislative responsibilities to manage heritage assets under their care and control. The subject works are noted as urgent so as to not further undermine the heritage fabric of the building.
- While the opportunities of the unique locality are noted, providing favourable conditions and certainty in the built form will assist negotiations for either a

small or substantial adaptation. This will provide the greatest flexibility in attracting a suitable tenant in a softer market period (as recommended by Colliers International Preliminary Commercial Assessment 2015).

Contributed Asset Policy 9.15

The City's Contributed Asset Policy provides guidance on the definition, recognition, disclosure and responsibility for the consideration of contributed assets and liabilities to be transferred to the City of Perth.

This policy applies to all physical assets contributed to the City of Perth, at no cost or for nominal consideration, including those acquired as a result of machinery-of-government changes and assets not previously recognised.

In this regard the Policy requires Council consideration of the matter and sets procedural requirements for financial reporting and assets registration upon receipt of the asset. The project is currently compliant with the procedural obligations outlined in the policy.

CONCLUSION:

As previously reported to and accepted by Council, the Pump Station acquisition would be for non-financial reasons and should be considered to be more in line with Council's leadership role, its interest in better activating Langley Park and the Swan River foreshore, as a heritage asset and in support of its existing facilities to the east: the playground; change rooms; public toilets; power and ancillary infrastructure.

It is therefore recommended that, subject to a satisfactory contribution from the Water Corporation, the City proceed to tender for the urgent and short term works to bring the site to a leasable standard and commence the Expressions of Interest process for the adaptive re-use.

Moved by Cr Adamos, seconded by Cr Yong

That Council:

- 1. notes the submissions received during the community consultation process for the adaptive reuse of the pump station building and the consensus for a café / dining premises inclusive of interpretative elements relating to the history of the building and the locality;***
- 2. notes the Department of Lands are entering the last stages of the reserve modification, pursuant to section 42(3) of the Land Administration Act 1997, via an extension to the Langley Park 'A Class' Reserve 12510, incorporating Reserve 13949, Lot 483 (Hill Street Water Pump Station No. 2), whereby the building and it's curtilage will come under the City's care control and management;***

(Cont'd)

3. *authorises the Chief Executive Officer to negotiate a contribution from Water Corporation towards;*
 - 3.1 *making the building safe for use;*
 - 3.2 *maintaining the heritage fabric and integrity of the State Heritage Listed (Ref. 03298) building in the short term; and*
 - 3.3 *bringing the building to a leasable standard including energising / servicing the site;*
- pursuant to the recommendations contained in sections 4.1 and 4.2 of the GHD Structural Condition Audit (2015) and the Colliers International Preliminary Commercial Assessment (2015);*
4. *approves the City of Perth undertaking an Expression of Interest process seeking a suitable tenant for the adaptive re-use of the pump station building and its curtilage, with:*
 - 4.1 *a focus on a 'destination' café / food and beverage use;*
 - 4.2 *a concept appropriately responsive to the heritage fabric of the building / locality; and*
 - 4.3 *a focus on activating the locality, ancillary to and in support of the Langley Park Reserve activities and adjacent City infrastructure;*
5. *approves the undertaking of a tender process pursuant to the works recommendations contained in sections 4.1 and 4.2 of the GHD Structural Condition Audit (2015) and the Colliers International Preliminary Commercial Assessment (2015);*
6. *notes a report will be provided to Council concurrently considering the proposals received under the Expressions of Interest process and the tender submissions for the proposed works as agreed with the Water Corporation.*

The motion was put and carried

The votes were recorded as follows:

For: Crs McEvoy, Adamos and Yong

Against: Nil

**PL231/15 HERITAGE GRANT APPLICATION FOR THE
PREPARATION OF A CONSERVATION MANAGEMENT
PLAN FOR THREE HOUSES, 55-59 GODERICH
STREET, EAST PERTH**

BACKGROUND:

FILE REFERENCE: P1023383
REPORTING UNIT: Arts, Culture and Heritage
RESPONSIBLE DIRECTORATE: Economic Development and Activation
DATE: 11 November 2015
MAP / SCHEDULE: Schedule 6 – Map and Photograph
Confidential Schedule 7 – Heritage Grant Application
(Distributed to Elected Members under a separate cover)

On 11 August 2015 the Council adopted the revised Council Policy 6.1 Heritage Grants (the Policy).

On 1 November 2015 the City opened the 2015/16 Heritage Grants Round. The Round closes on 30 January 2015. In accordance with Clause 21 of the Policy applications are to be determined by the Council.

On 2 November 2015 the City received a heritage grant application from the owners of the place known as *Three Houses* at 55-59 Goderich Street, East Perth (Schedule 6 and Confidential Schedule 7). The application seeks full funding of \$11,996 for the preparation of a conservation management plan for the property. A conservation management plan is the principal guiding document for the conservation management of a heritage place.

At the request of the three owners, on 22 September 2015 the Council included 55-59 Goderich Street, East Perth in the City Planning Scheme No.2 Register of Places of Heritage Significance.

The subject property comprises three individually titled strata properties that together contain a row of adjoining single storey brick terrace houses constructed circa 1880 in the Victorian Georgian architectural style.

Details about the City's Heritage Grant Program and the application received are detailed in this report.

LEGISLATION / STRATEGIC PLAN / POLICY:

| | | |
|---|--------------------------------|---|
| Legislation | City Planning Scheme No. 2. | |
| Integrated Planning and Reporting Framework Implications | Corporate Business Plan | |
| | S7 | Collaborate with private sector to leverage |
| | S9 | Promote and facilitate CBD living. |
| | 9.2 | Review the City's approach to Conservation of |

- Heritage Places.
- S15 Healthy and Active in Perth
- 15.1 Undertake a full review of the Grants, Donations, Sponsorships and Event Funding Policies

Strategic Community Plan

Council Four Year Priorities: Community Outcome

Healthy and Active in Perth.

A city with a well-integrated built and green environment in which people and close families chose a lifestyle that enhances their physical and mental health and take part in arts, cultural and local community events.

Policy

Council Policy

6.1 – Heritage Grants

DETAILS:**Heritage Grants Program**

The City's program of development and financial incentives is aimed at encouraging and assisting landowners to retain, maintain, conserve and use heritage places. Heritage Grants are a key component of the program and are primarily focused on the conservation, rather than maintenance, of heritage places.

The objective of Council Policy 6.1 Heritage Grants (Council Policy 6.1) is to encourage and assist landowners to conserve and continue the active use of heritage places.

Matched funding up to \$40,000 is available for works associated with the conservation of heritage places, and up to \$20,000 for the preparation of studies, reports or advice prepared to inform the future retention, conservation and use of a heritage place. Full funding to a maximum amount of \$20,000 is also available for the preparation of a Conservation Management Plan for a heritage place.

To be eligible for the City's heritage grants the property must be rateable, heritage listed, and included within the City's Local Government Area.

Applications for the 2015/16 Heritage Grant Round opened on 1 November 2015 and will close on 30 January 2016. In accordance with Council Policy 6.1 all applications must be determined by the Council, and the Council may prioritise or place greater weight on any of the assessment criteria.

Heritage Grant – Application

As noted above, the heritage grant application for the place known as *Three Houses* at 55-59 Goderich Street, East Perth seeks full funding of \$11,996 for the preparation of a conservation management plan (CMP) for the property (Confidential Schedule 7).

In accordance with the application requirements set out in Council Policy 6.1 the application comprises the following:

1. A completed 'Heritage Grant Application';
2. Applicant declaration;
3. Evidence of authority to act on behalf of the property landowners;
4. A succinct current property condition report;
5. A project scope;
6. An itemised budget;
7. Three quotes; and
8. Evidence of full value building insurance.

Heritage Grant – Assessment

The subject property meets the eligibility requirements set out in Council Policy 6.1, which require the applicant to be the landowner (or lawfully act on behalf of the landowner) of a rateable property that is listed as a Heritage Place in the City of Perth City Planning Scheme. In this regard it should be noted that the property was included in the Heritage Register at the request of all three landowners.

In accordance with Council Policy 6.1, the application has been assessed against the following relevant criteria:

| No. | Assessment Criteria | Applicant's Statement Addressing Selection Criteria | Officer Comment |
|------|---|--|---|
| 19.1 | There is a demonstrated need for a property management tool to guide future change and inform effective decisions in relation to change in a heritage place, specifically through conservation and maintenance schedules. | <p>The place occupies 28 meters of street frontage to Goderich Street and therefore has a significant impact on the streetscape.</p> <p>The facades have varying treatments in relation to colour schemes and architectural details.</p> <p>A CMP would guide future change in a more coordinated and consistent manner, that will conserve the existing fabric and enhance the heritage streetscape.</p> <p>As the place has three owners, a CMP will serve as an essential management tool for coordinated decision making in relation to alterations, conservation and maintenance.</p> | <p>A CMP will help ensure that the future maintenance and proposals are properly planned and managed to ensure the cultural heritage significance of the place is maintained.</p> <p>A CMP for this property will help guide the owners to undertake the desired conservation of the place.</p> |
| 19.2 | A revised/updated CMP is required. | Funding is for the first CMP for the property. | There is currently no CMP for the |

| No. | Assessment Criteria | Applicant's Statement Addressing Selection Criteria | Officer Comment |
|-------|---|--|--|
| | | | place. |
| 19.3. | Whether any development based incentives received for the heritage place. | No other development based incentives have been received for the properties. | No other development based incentives have been received for the properties. |
| 19.4. | Whether any other funding received or sought. | No other funding has been received or sought. | No other funding has been received or sought. |

Heritage Grant – Funding Amount

It is noted that all three quotes were prepared by suitability qualified professionals and in accordance with the State Heritage Office's guidelines for preparing Conservation Management Plans.

Given that all three quotes were prepared by qualified professionals to the same brief, the funding amount reflects the lowest quote based on best value for money.

It should be noted that the Council has approved 26 CMPs since 2003, and has never refused an eligible application for a CMP.

The funding amount sought is consistent with previously approved fully funded heritage grants awarded by the Council for CMPs for similar single storey places including, \$8,000 for 33 Wellington Street, East Perth (2008) and \$13,000 for 122 Aberdeen Street, Northbridge (2010).

Heritage Grant – Application Process

Whilst the grant round is currently open, the application has been progressed to the Council prior to the current grant round because it seeks funding for a CMP, rather than works.

As general heritage conservation principle, a CMP should be prepared for a heritage place before undertaking work as it should be used to inform future works. A CMP helps to determine value and effects of changes to a heritage place, and includes policy to guide change which can be used to assess development applications.

Given this, it is considered appropriate that the subject application be progressed prior to the closure of the current heritage grant round. This will allow the applicant to progress the preparation of the CMP and plan for subsequent conservation work. It is understood that the owners may seek further heritage grant funding for conservation works to reinstate the appearance of the original façade. It is also noted that a CMP is a prerequisite of heritage grant funding that exceeds \$20,000.

In accordance with Council Policy 6.1 the applicant must enter into a Heritage Grant Funding Agreement with the City that includes conditions pertaining to the grant

funding. For CMPs it is a requirement that one digital copy of the CMP must be provided to the City prior to the payment of funds, and that the author must grant to the City in writing perpetual, non-exclusive licence to copy, display and electronically retain the document.

Within six months from the project completion, and no later than 30 months from the date of the Council approved application, a written Acquittal Report for the project must be submitted to the City. The report must demonstrate how the recommendations have been, or are intended to be, implemented. Payment will only be made following acceptance by the City of the written acquittal report by the City.

FINANCIAL IMPLICATIONS:

| | |
|-----------------------|-----------------|
| ACCOUNT NO: | CL 1620400 |
| BUDGET ITEM: | Heritage Grants |
| BUDGET PAGE NUMBER: | 10 |
| BUDGETED AMOUNT: | \$400,000 |
| AMOUNT SPENT TO DATE: | \$ 0 |
| PROPOSED COST: | \$ 11,996 |
| BALANCE: | \$388,004 |

The balance of the 2015/16 Heritage Grants budget will be available for any other heritage grant applications received by the 30 January 2016, subject to Council approval.

COMMENTS:

The heritage grant application for full funding of \$11,996 for the preparation of a CMP for the heritage listed *Three Houses* at 55-57 Goderich Street will provide the owners of the place with essential information on the history, significance, physical condition, recommendations for works aimed at conserving and maintaining heritage fabric.

The application meets the requirements of Council Policy 6.1, specifically eligibility and application requirements. The application addresses the relevant assessment criteria as set out in Council Policy 6.1

Whilst the current heritage grant round has yet to close, the application is presented to the Council given that it is for the preparation of a CMP rather than works. In this regards, it should be noted that since 2002 the City has approval all eligible application for full funding of CMPs. As noted above, the applicant will be required to enter into a Funding Agreement and will not receive payment until the City has accepted the written Acquittal report.

Moved by Cr McEvoy, seconded by Cr Adamos

That Council approves the heritage grant application from the owners of 55-57 Goderich Street, East Perth for \$11,996 for the preparation of a Conservation Management Plan in accordance with Council Policy No 6.1 Heritage Grants.

The motion was put and carried

The votes were recorded as follows:

For: Crs McEvoy, Adamos and Yong

Against: Nil

PL232/15 AMENDMENT – PROCLAMATION CHANGES TO ROADS WITHIN THE NARROWS INTERCHANGE

BACKGROUND:

| | |
|--------------------------|---|
| FILE REFERENCE: | P1003011 |
| REPORTING UNIT: | Transport |
| RESPONSIBLE DIRECTORATE: | Planning and Development |
| DATE: | 23 November 2015 |
| MAP / SCHEDULE: | Schedule 8 – Existing Narrows Interchange Proclamation (Reference No. 0921-0188-02) Schedule 9 – For endorsement - Narrows interchange proclamation (Reference No. 0921-0188-03) |

The Council at its meeting on **26 June 2012** resolved to:

“endorse the revised proclamation of roads at the Mitchell Freeway (northbound) off-ramps to Mounts Bay Road as shown in Maps B and C.”

Further to the Council's support in relation to the proclamation of roads in the vicinity of the Narrows Interchange, the City received a request from Main Roads WA on 15 January 2015 to amend the proclamation, deproclaim and renumber various sections of road within the Narrows interchange.

The Commissioner of Main Roads WA intends to make a recommendation to the Minister for Transport to make the changes shown on Schedule 9 (Reference No. 0921-0188-03).

LEGISLATION / STRATEGIC PLAN / POLICY:

| | |
|---|---|
| Legislation | Section 13 of the <i>Main Roads Act 1930</i> |
| Strategic Plan | Movement, Transport and Parking Plan the integration of movement options for the city incorporating all modes of transport, including walking and cycling |
| Integrated Planning and Reporting Framework Implications | Strategic Community Plan Council Four Year Priorities: Getting Around Perth S3 Proactive planning for an integrated transport system, including light rail, that meets community needs and makes the sustainable choice the easy choice. |

DETAILS:

The effect of the proclamation will be as following:

Deproclaim

- H916, a section of Riverside Drive between Barrack Street and William Street;
- H508 and H510, sections of slip lane from Riverside Drive onto the Mitchell Freeway south or Point Lewis Rotary.

Proclaim

- H508, an amended road section between Riverside Drive (between Point Lewis Rotary and William Street) and Mitchell Fwy (south);
- H509, a new road section between Point Lewis Rotary and William Street;
- H510, a new road section between Riverside Drive (between Point Lewis Rotary and William Street) and Mitchell Fwy (south);
- 124Z028, a renumbering amended PTA road section from Mitchell Fwy (north) to the southern entrance to The Esplanade Bus Port;
- 124Z029, a renumbering amended PTA road section from the southern entrance to The Esplanade Bus Port to Mitchell Fwy (south).

The management responsibility for the above changes will remain with Main Roads WA. Footpaths will be excluded from the proclamation and as such remain the responsibility of the City of Perth.

FINANCIAL IMPLICATIONS:

There are no financial implications related to this report.

COMMENTS:

In order to present the recommendation to the Minister for Transport to reproclaim various road or road sections within the Narrows Interchange, the Commissioner of Main Roads WA requires the endorsement of the Council of Schedule 9 attached.

The proposed deproclamation and proclamations will not change the status or responsibility for any of the road sections relating to this report. It is purely an administrative matter to align the proclamation with the roads surrounding the Narrows Interchange as they exist now.

Moved by Cr McEvoy, seconded by Cr Yong

That Council endorses various changes to the proclamation of roads within the Narrows interchange as shown in Schedule 9.

The motion was put and carried

The votes were recorded as follows:

For: Crs McEvoy, Adamos and Yong

Against: Nil

**PL233/15 21 – 23 (LOTS 130 AND 131) COLIN STREET, WEST
PERTH – PROPOSED TWO LEVEL RESTAURANT
INCLUDING TWO CAR PARKING BAYS**

BACKGROUND:

| | |
|--------------------------|--|
| SUBURB/LOCATION: | 21 – 23 Colin Street, West Perth |
| FILE REFERENCE: | 2015/5206 |
| REPORTING UNIT: | Development Approvals |
| RESPONSIBLE DIRECTORATE: | Planning and Development |
| DATE: | 24 November 2015 |
| MAP / SCHEDULE: | Schedule 11 - Map and colour perspective for 21 – 23 Colin Street, West Perth |
| 3D MODEL PRESENTATION: | A 3D Model for this application will be available at the Committee meeting. |
| LANDOWNER: | Salwell Property Pty Ltd |
| APPLICANT: | Matthews and Scavalli Architects |
| ZONING: | (MRS Zone) Urban (City Planning Scheme Precinct) West Perth (P10) (City Planning Scheme Use Area) Office / Residential |
| APPROXIMATE COST: | \$800,000 |

SITE HISTORY:

The 645m² site is located on the western side of Colin Street, south of the intersection of Colin and Ord Streets in West Perth. The subject site is occupied by a single storey building that has previously been used for office purposes. There is an existing right of way to the rear of the property.

DETAILS:

Approval is sought for the demolition of the existing single storey building and the construction of a two level restaurant building including two car parking bays to the rear of the proposed building at 21 – 23 Colin Street, West Perth.

The proposed development includes the following:

| | |
|---------------------------|--|
| Ground floor level | This level consists of a landscaped semi-covered alfresco dining area within the front setback area fronting Colin Street, a front bar area, a seated restaurant area, a kitchen, a staff room with lockers and toilet, a stair case with a bin store area underneath and two car parking bays with vehicular access off the adjoining rear laneway. |
| First floor level | This level contains two seated dining areas for the restaurant, lift and stair access, bathroom facilities and an alfresco dining area to the rear of the building. |

The proposed building will be constructed of concrete panels, textured masonry, aluminium feature cladding in Bronze colour to the feature boxes, glazed bi fold doors in black to Colin Street, black louvres, and a retractable shading device to the first floor windows facing north and Colin Street. The proposed landscaping plan for the north-east facing alfresco dining area will feature in-built tables and fixed timber seating, concrete planters and paving and a selection of low shrubs, climbers and small to medium trees.

LEGISLATION / POLICY:

Legislation *Planning and Development Act 2005*
 City Planning Scheme No.2
 Planning and Development (Local Planning Schemes)
 Regulations 2015 – Deemed Provisions for Local Planning
 Schemes

Policy 5.1 - Parking Policy
 5.3 – Bicycle Parking and End of Journey Facilities

COMPLIANCE WITH PLANNING SCHEME:**Land Use**

The subject site is located in the Office / Residential use area of the West Perth Precinct (P10) under City Planning Scheme No. 2 (CPS2). The Statement of Intent for the precinct states that the area will provide for a range of office and residential

activities together with restaurants (including cafes) and other uses which serve the immediate needs of the work force and residents.

'Dining' is a 'Preferred' ('P') uses in the Office / Residential use area of the West Perth Precinct.

Development Requirements

Buildings within the Office/Residential use area shall be set in landscaped surrounds to create an open spacious character quite distinct from the continuous built edge of the primary office district in the city centre. Buildings shall be well set back from boundaries and evoke a sense of prestige, which should be further enhanced by permanent, in ground landscaping.

The proposal's compliance with the CPS2 development requirements is summarised below for the proposed site:

| Development Standard | Proposed | Required/Permitted |
|----------------------|--|-------------------------------|
| Maximum Plot Ratio | 1.03:1 (675m ²) | 1.33:1.0 (867m ²) |
| Building height | 10.8 metres | 29 metres (maximum) |
| Setbacks | | |
| Colin Street | | |
| - Ground Floor Level | 11 metres | 4.5 metres (minimum) |
| - First Floor Level | 4.5 metres | 4.5 metres (minimum) |
| Side (North) | | |
| - Ground Level | Nil (for 17 metres of boundary) | 3.5 metres (minimum) |
| - First floor level | Nil (for 17 metres of boundary) | 3.5 metres (minimum) |
| Side (South) | | |
| - Ground level | Nil (26 metres of boundary) | 3.5 metres (minimum) |
| - First floor level | Nil (35 metres of boundary) | 3.5 metres (minimum) |
| Rear (West) | | |
| - Ground Floor Level | 3 metres | 3 metres (minimum) |
| - First floor level | 3 metres | 3 metres (minimum) |

| Development Standard | Proposed | Required/Permitted |
|-----------------------------|--------------------------|--|
| Car Parking - Commercial | 2 bays | 16 bays (maximum) |
| Bicycle Parking | Nil | 1 bay (minimum) |
| Landscaping | 21% (137m ²) | 25% of the site area (163m ²) |

Variations to the side setback, landscaping and bicycle parking provisions applicable to the development can be granted by an absolute majority decision of the Council, in accordance with Clause 47 of the CPS2 and provided the Council is satisfied that:

'47(3)(c)(i) if approval were to be granted, the development would be consistent with:

- (A) the orderly and proper planning of the locality;*
- (B) the conservation of the amenities of the locality; and*
- (C) the statement of intent set out in the relevant precinct plan; and*

(ii) the non-compliance would not have any undue adverse effect on:

- (A) the occupiers or users of the development;*
- (B) the property in, or the inhabitants of, the locality; or*
- (C) the likely future development of the locality'.*

COMMENTS:

Consultation

The application was advertised to surrounding land owners for comment for a period of 14 days due to the proposed variations to the setback and landscaping requirements of the CPS2. No submissions were received during this period.

Design Advisory Committee

The application was referred to the Design Advisory Committee (DAC) at its meeting held on 19 November 2015 for comment with regard to the design of the proposed two storey development. The DAC advised that it supports the development however notes and advises that:

- "1. In terms of general design and finishes raises concern in regard to retaining the proposed quality of materials including the concrete form finish noting the low budget for the proposed development;*
- 2. further details of the proposed high quality finishes for the development, particularly to the boundary walls and walls within the landscaped area and setback areas, should be submitted for consideration and then followed through and not modified during the permit or construction phases;*
- 3. the side setback variations are supported noting that the height of two levels provides considerable sky views compared to an alternative compliant tower*

scheme, however considers that there is an opportunity for the extent of the southern boundary wall to be reduced both at the front and the rear of the site thereby reducing its impact;

4. *the variations to the landscape provisions are supported on the basis of the proposed design of the front and side landscape and open areas, subject to further details of the proposed planting being submitted."*

The applicant has provided a detailed response to each of the DAC's comments. In response to items 1 and 2, the applicant has provided an elevation of the building providing a more clearly defined concept of proposed materials. The applicant has stated that 'the proposal seeks to present a well executed development which is simple in its palette selection as the restaurant 'fabric' of patrons and furnishings will add a further level of detail.' The selection of colours and materials shown on the plans received 27 November 2015 is considered to be of a high quality appropriate for the West Perth precinct, however it is recommended that any approval be conditioned to ensure the quality of materials and colours are maintained.

In response to item 3, the applicant has advised that they have reviewed the southern elevation and reduced the extent of the walls at ground level at both the front and rear of the site in accordance with the recommendation of the DAC.

The applicant has provided further details of the proposed landscaping plan showing a selection of predominantly ground covers and low shrubs, strappy plants for their ability to be viewed through, and a selection of small to medium trees which have been selected due to their 'architectural qualities'. Details of the proposed concrete paving within the alfresco dining area, large planters as well as timber benches have also been provided. The provision of extensive in ground landscaping, fixed bench seating and use of different materials within the front setback will provide an attractive street frontage that is considered consistent with the design intent for West Perth and is therefore supported. Any approval should be conditioned to ensure the proposed landscaping is completed in accordance with the landscaping plan.

Setbacks

The building complies with the minimum 4.5 metre front setback prescribed under the CPS2, with the ground floor level setback further to provide a landscaped alfresco dining area along the street front and northern portion of the site. The applicant has amended the application in response to the DAC's comments, reducing the extent of the southern wall on the ground level to the front and rear of the property to reduce the impact of the wall on adjoining properties and the streetscape.

The application proposes variations to both side setback provisions of the CPS2. A nil side setback is proposed for the 17 metres of the rear portion of the building on the northern lot boundary, with the front half of the building being setback 7 metres at ground level and 3.5 metres at the first floor level from the front boundary. A nil side setback is also proposed for 26 metres of the ground floor level and 35 metres of the first floor level of the building.

The increased setback of the ground floor level from the street and the provision of an alfresco dining area and in ground landscaping helps create an open spacious character which is consistent with the statement of intent of the West Perth precinct. The DAC supported the application on the basis that the proposed development is limited to two levels and therefore provides good access to natural daylight between buildings. It is recommended that the proposed variations to the side setbacks be supported on this basis, and giving consideration to the requirements of Clause 47 of the CPS2.

Landscaping

The application seeks a minor variation to the minimum 25% landscaping requirement of the CPS2. A total of 21% or 137m² of in ground landscaping is proposed for the site. The applicant has provided a detailed landscaping plan showing a selection of low and medium sized plants and trees which have been selected to ensure visibility into the site is maintained and also for their 'architectural qualities'. The proposed landscaping is located within the front setback area which is consistent with the scheme requirement for more than 50% of the front setback area of narrow lots to comprise of high quality in ground landscaping within the West Perth precinct. It is recommended that the proposed variation to the minimum landscaping be supported.

Conclusion

The proposed development will contribute to commercial activities in the West Perth Precinct. Although the proposal seeks variations to the side setback and landscaping provisions of the scheme, the proposal generally satisfies the design intent for the West Perth Precinct. It is considered that the development will not have an adverse impact on the streetscape, the adjoining properties or on the amenity of the locality. Having regard to the above it is recommended that the proposal be supported subject to relevant conditions.

OFFICER RECOMMENDATION:

That, in accordance with the provisions of City Planning Scheme No. 2, the Planning and Development (Local Planning Schemes) Regulations 2015 – Deemed Provisions for Local Planning Schemes and the Metropolitan Region Scheme, the Council APPROVES BY AN ABSOLUTE MAJORITY the application for demolition of the existing single storey building and the construction of a two level restaurant building including two car parking bays at 21 - 23 (Lots 130 and 131) Colin Street, West Perth, as detailed on the Metropolitan Region Scheme Form One dated 24 April 2015, and as shown on the plans received 9 September 2015 and 27 November 2015 subject to:

1. final details and a sample board of the high quality materials, colours and finishes for the proposed building being submitted and approved by the City prior to applying for a building permit;
2. a detailed landscaping and reticulation plan, demonstrating in-ground landscaping in the front setback and the use of mature species that are suitable

in low light or shaded areas to the undercroft landscaped areas, to create an established landscaped presentation to Colin Street, being submitted and approved by the City prior to applying for a building permit, with the approved landscaping being installed prior to the occupation of the building and thereafter maintained to a high standard;

3. a maximum of two car parking bays being provided on-site, with all bays being for the exclusive use of tenants or occupants of the development and not being leased or otherwise reserved for use of tenants or occupants of other buildings or sites;
4. the dimensions of all car parking bays, aisle widths and circulation areas complying with the Australian Standard AS/NZS 2890.1/2004;
5. the proposed pedestrian entry adjacent to Colin Street being consistent with the existing footpath levels immediately adjacent to the entry with final details of the existing and proposed levels being submitted and approved by the City prior to the submission of an application for a building permit;
6. any signage for the proposed restaurant being designed as an integral part of the development and any signs not exempt from approval under the City's Planning Policy 4.6 – Signs being subject to a separate application for approval; and
7. the submission of a construction management plan for the proposal detailing how it is proposed to manage:
 - 7.1 the delivery of materials and equipment to the site;
 - 7.2 the storage of materials and equipment on the site;
 - 7.3 the parking arrangements for the contractors and subcontractors;
 - 7.4 the impact on traffic movement; and
 - 7.5 other matters likely to impact on the surrounding properties.

The Planning Committee agreed to amend Part 1 of the Officer Recommendation as follows:

1. *final details and a sample board of the high quality materials, colours and finishes for the proposed building, with particular attention to the southern elevation, being submitted and approved by the City prior to applying for a building permit;*

PRIMARY MOTION AS AMENDED was put

That, in accordance with the provisions of City Planning Scheme No. 2, the Planning and Development (Local Planning Schemes) Regulations 2015 – Deemed Provisions for Local Planning Schemes and the Metropolitan Region Scheme, the Council APPROVES BY AN ABSOLUTE MAJORITY the application for demolition of the existing single storey building and the construction of a two level restaurant building including two car parking bays at 21 - 23 (Lots 130 and 131) Colin Street, West Perth, as detailed on the Metropolitan Region Scheme Form One dated 24 April 2015, and as shown on the plans received 9 September 2015 and 27 November 2015 subject to:

- 1. final details and a sample board of the high quality materials, colours and finishes for the proposed building, with particular attention to the southern elevation, being submitted and approved by the City prior to applying for a building permit;***
- 2. a detailed landscaping and reticulation plan, demonstrating in-ground landscaping in the front setback and the use of mature species that are suitable in low light or shaded areas to the undercroft landscaped areas, to create an established landscaped presentation to Colin Street, being submitted and approved by the City prior to applying for a building permit, with the approved landscaping being installed prior to the occupation of the building and thereafter maintained to a high standard;***
- 3. a maximum of two car parking bays being provided on-site, with all bays being for the exclusive use of tenants or occupants of the development and not being leased or otherwise reserved for use of tenants or occupants of other buildings or sites;***
- 4. the dimensions of all car parking bays, aisle widths and circulation areas complying with the Australian Standard AS/NZS 2890.1/2004;***
- 5. the proposed pedestrian entry adjacent to Colin Street being consistent with the existing footpath levels immediately adjacent to the entry with final details of the existing and proposed levels being submitted and approved by the City prior to the submission of an application for a building permit;***
- 6. any signage for the proposed restaurant being designed as an integral part of the development and any signs not exempt from approval under the City's Planning Policy 4.6 – Signs being subject to a separate application for approval; and***

(Cont'd)

7. the submission of a construction management plan for the proposal detailing how it is proposed to manage:

7.1 the delivery of materials and equipment to the site;

7.2 the storage of materials and equipment on the site;

7.3 the parking arrangements for the contractors and subcontractors;

7.4 the impact on traffic movement; and

7.5 other matters likely to impact on the surrounding properties.

Moved by Cr Yong, seconded by Cr McEvoy

The motion was put and carried

The votes were recorded as follows:

For: Crs McEvoy, Adamos and Yong

Against: Nil

Reason: The Planning Committee considered that the amendment to part 1 of the Officer Recommendation provides clarity for the applicant.

PL234/15 MOTIONS OF WHICH PREVIOUS NOTICE HAS BEEN GIVEN

Nil

PL235/15 GENERAL BUSINESS

Responses to General Business from a Previous Meeting

Nil

New General Business

Nil

PL236/15 ITEMS FOR CONSIDERATION AT A FUTURE MEETING

Outstanding Items:

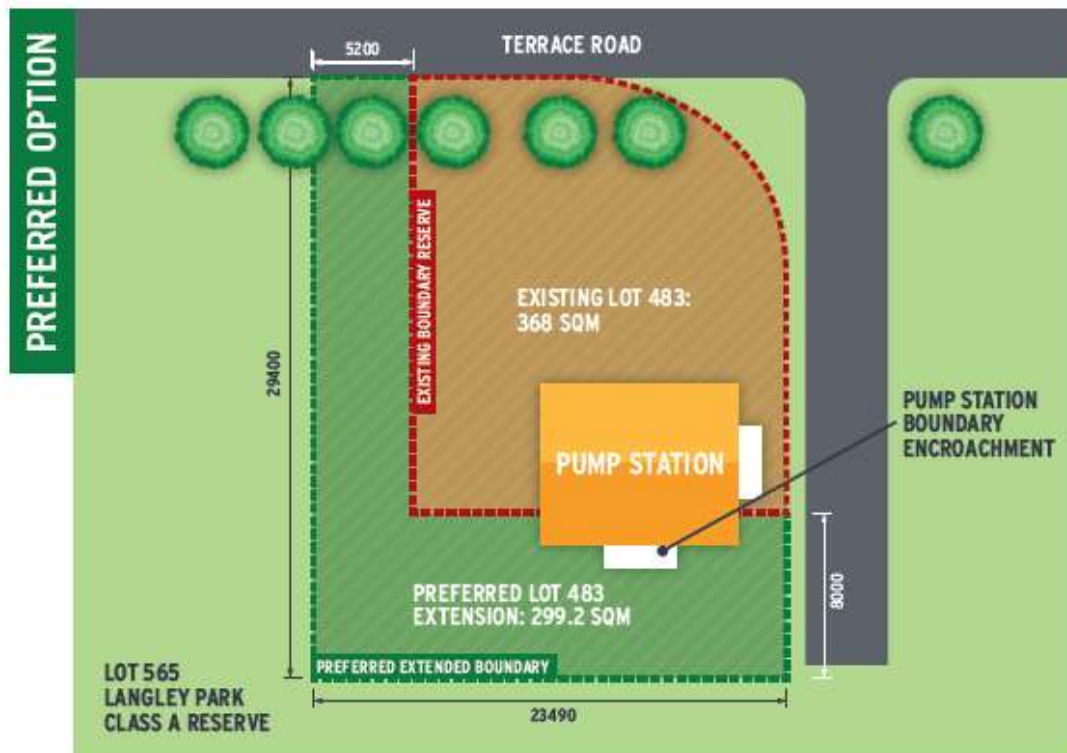
Nil

PL237/15 CLOSE OF MEETING

6.08pm There being no further business the Presiding Member declared the meeting closed.

**SCHEDULES
FOR THE MINUTES OF THE
PLANNING COMMITTEE
MEETING HELD ON
8 DECEMBER 2015**

BOUNDARY RATIONALISATION



SCHEDULE 2

Schedule of Submissions

| Respondent | Respondent's Comment | COP's Comment |
|------------------|--|-----------------------------------|
| 1. Soliman Allam | a) A classy Café / Restaurant in this area will do a difference. | a) Noted. |
| 2. Richard Ng | <p>a) Whilst preserving the heritage fabric of the building, renovate the pump station and making it into a charming old-world Australian-style cafe that serves ONLY Australian fare.</p> <p>No fast foods, no imported foodstuff. Everything ought to be Australian produced to preserve the "heritage" local brand. Something Australians can be proud of. After all, it is the "heritage" theme that will be the driver.</p> <p>Combine that heritage cafe with an adjoining section that sells WA wines from the Margaret River and Swan Valley regions (for the benefit of foreign visitors staying close by at hotels and guest houses).</p> <p>Both suggestions will go down well with local Australians as well as foreign visitors who stay in the vicinity.</p> <p>b) Day and night, this redesigned heritage building, transformed from a pump station into a cafe and winery, not only will attract all and sundry who stroll along Langley Park, but will also create a useful revenue stream for the City of Perth as managers of the facility.</p> | <p>a) Noted.</p> <p>b) Noted.</p> |

SCHEDULE 2

| | | |
|-----------------------------|--|-----------|
| | <p>There are many Australians who make use of Langley Park every day, especially on weekends. There are numerous community events and social activities that take place throughout the year at Langley Park.</p> <p>There are also many foreign visitors staying in guest accommodation nearby (Pan Pacific Hotel, Novotel, Duxton etc) who stroll through Langley Park.</p> <p>The suggestions above, if implemented, would enhance the enjoyment of Langley Park for all users with the added dimension of being able to encourage those living outside the City of Perth to visit with family and friends. Day and night...and spend money too!</p> | |
| 3. Harry and Yvonne Marwick | a) We think that the pumping station would make a great themed café/coffee shop, which could act as a pseudo museum or at least a reminder of the early airfield and what the foreshore looked like through the city's evolution. | a) Noted |
| 4. Craig Heath | <p>a) I thought this building would make a great Cafe I am sure it would attract customers from the nearby apartments, joggers and cyclists along with events and sporting activities held at Langley Park</p> <p>It could be named along with its historic use (Station No 2 Cafe) (1915 Station Cafe) you get the idea.</p> | a) Noted |
| 5. V Ioppolo & K Rimmer | a) We see the concept to be quite favourable and one which would add value on several levels. | a) Noted. |

| | | |
|----------------|---|--|
| | <p>b) One of the issues to be addressed however relates to funding. We would not like to see our rates increased as a result of the proposed commitment. Can you please advise how the works, management and ongoing operation/maintenance will be funded. We presume there would be a generous contribution from the Water Corporation</p> | <p>b) Noted.</p> |
| 6. Mark Fisher | <p>a) I would like to see something setup along the lines of what has been accomplished by the Shire of Kalamunda/Lesmurdie with the Zig-Zag Historical Centre and Café.</p> <p>b) Depending on budget allocation and astute planning, I believe the Water Corporation and the City of Perth could establish this location as a note-worthy local historical site and café.</p> <p>c) I propose to make it a place where people could visit a significant display of our early local history and a location where all visitors could share a coffee and/or breakfast or lunch all year round.</p> | <p>a) Noted.</p> <p>b) Noted.</p> <p>c) Noted.</p> |
| 7. John Kelly | <p>a) My suggestion is that the premises could be given to the National Trust for the purpose of mounting an early settlement history display for the area, for limited public viewing.</p> <p>An example of a similar small building used for such purposes can be found just inside the entry gates of the Hobart Botanical Gardens that displays photos, art work and written stories of the history of the garden.</p> <p>b) This area lends itself to a similar display being on the site of the first aerodrome and close to the city and such historic buildings as Government House</p> | <p>a) Noted.</p> <p>b) Noted.</p> |

| | | |
|--|---|-----------------------------------|
| | and so on. The changes to the Swan River shore line could also be featured. | |
| 8. Lorraine J Kennedy | a) The City of Perth could utilise this heritage listed building, and offer some shade and seating around the building. | a) Noted. |
| 9. Michael John Guerin | <p>a) My thoughts for consideration are: convert the existing building into a mini museum showing the history of Langley Park. This could include: Historical records & photographs, which could either be arranged inside the building, incorporated within a scrolling device or closed circuit TV. Perhaps a combination? Include more recent events such as: the Red Bull Air Race, The Giants, and Circuses etc. Highlight regular sporting events and everyday use.</p> <p>b) Perceived benefits may be an increased appreciation of the wonderful asset that Langley Park is to the City of Perth, and presumably be of low maintenance cost once completed.</p> | <p>a) Noted.</p> <p>b) Noted.</p> |
| 10. Bob Welsh - Event Compliance Officer | a) TriEvents is a regular user of Langley Park and we would like to see the building being made available to users as an Event Admin Centre or Headquarters on Event Days. Under current arrangement we bring a temporary site shed or marquee, depending on the budget and nature of the event. To have access to a facility that is maintained for this purpose would be another improvement in our event organisation. No doubt there would need to be some expenditure on having the facility prepared for community use, but this will enhance the quality of event conducted on the park. I look forward to | a) Noted. |

| | | |
|---------------------|---|---|
| | further comment at the forthcoming public forum. | |
| 11. Warren Naseby | <p>a) This is a very solid, beautiful old building, suffering from years of neglect and some vandalism. The basic structure looks good to my unprofessional eye so it would lend itself to a variety of uses.</p> <p>b) The most obvious (and personally desired) use would be as a restaurant/café. This area - the general Langley Park vicinity - suffers from a severe lack of options for park users.</p> <p>c) You need to leave the park to purchase coffee/ food of any reasonable quality. This is not acceptable for a major city location. There are many events held in the park and if there are no vans on site people have no options. Parents attending a sporting event with their children cannot safely leave the park to, say, buy a coffee.</p> <p>d) On the point of food vans, I have only seen these in Langley Park once when there hasn't been a major event on. Casual park users are usually out of luck.</p> <p>e) If we are to maximise the amenity of this major and unique asset, an effort must be made to attract users and keep them in the park area. Access to quality and convenient food and drinks would be a major component in achieving this goal.</p> | <p>a) Noted.</p> <p>b) Noted.</p> <p>c) Noted.</p> <p>d) Noted. The food truck permits address the circumstances around conflicting events though the established process.</p> <p>e) Noted.</p> |
| 12. Kerrie Fletcher | <p>a) I have always thought it is such a waste that nothing has been done with this site.</p> <p>b) My partner and I feel that amenities along Langley Park are lacking, i.e. it would be fantastic if the site were to be transformed into a coffee shop / cafe etc.</p> <p>c) Living by Langley Park we notice a lot of activity on</p> | <p>a) Noted.</p> <p>b) Noted.</p> <p>c) Noted.</p> |

| | | |
|------------------|--|-----------|
| | the Park. There a lot of residents in the high rise buildings and we feel this site would be well utilise as a coffee shop / cafe and would have sufficient patronage to be financially viable. | |
| 13. Nathan Smith | a) I live in the build opposite the Pump House Station. I would love to see it renovated/ renewed and to include a large BBQ area. A large BBQ area would entice people to use the park more. There are currently no BBQ facilities on Langley Park. You would need to have multiple BBQ's though. Make it a large area and show piece the pump station. | a) Noted. |



City of Perth

Langley Water Pump Station Consultation Outcomes Report

September 2015

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Appendices

- Appendix A – Presentation
- Appendix B – Workshop Notes

1. Introduction

The City of Perth engaged GHD to facilitate a public forum regarding the adaptive reuse of the Langley Park Pump station, which is located at the southern end of Hill Street, Perth. The following report provides an overview of the public forum and commentary relating to the options discussed for the potential future use of the Langley Park Pump station.

1.1 Background

The City of Perth is set to acquire the existing Langley Park pump station as a result of a Water Corporation request to transfer the existing Management Order for the site to the City of Perth. The City of Perth seeks to optimise the future use of the pump station, which has been disused since its decommissioning in 1989.

As a precursor to the transfer of the Management Order, the City of Perth has initiated a boundary re-alignment in order to rationalise the existing boundary configuration to include the pump station into a single lot, and also to extend the boundary further south and west, to enable a more useable area surrounding the pump station.

Public comment was sought with regard to what extent the boundary should be shifted, with the preferred option shown in Figure 1 below. The southern boundary was nominated to be extended by 8 metres from the original boundary line, in order to align with the adjacent change rooms, located to the east, and 5.2 metres to the west, to create a more useable space for the adaptive re-use of the pump station.

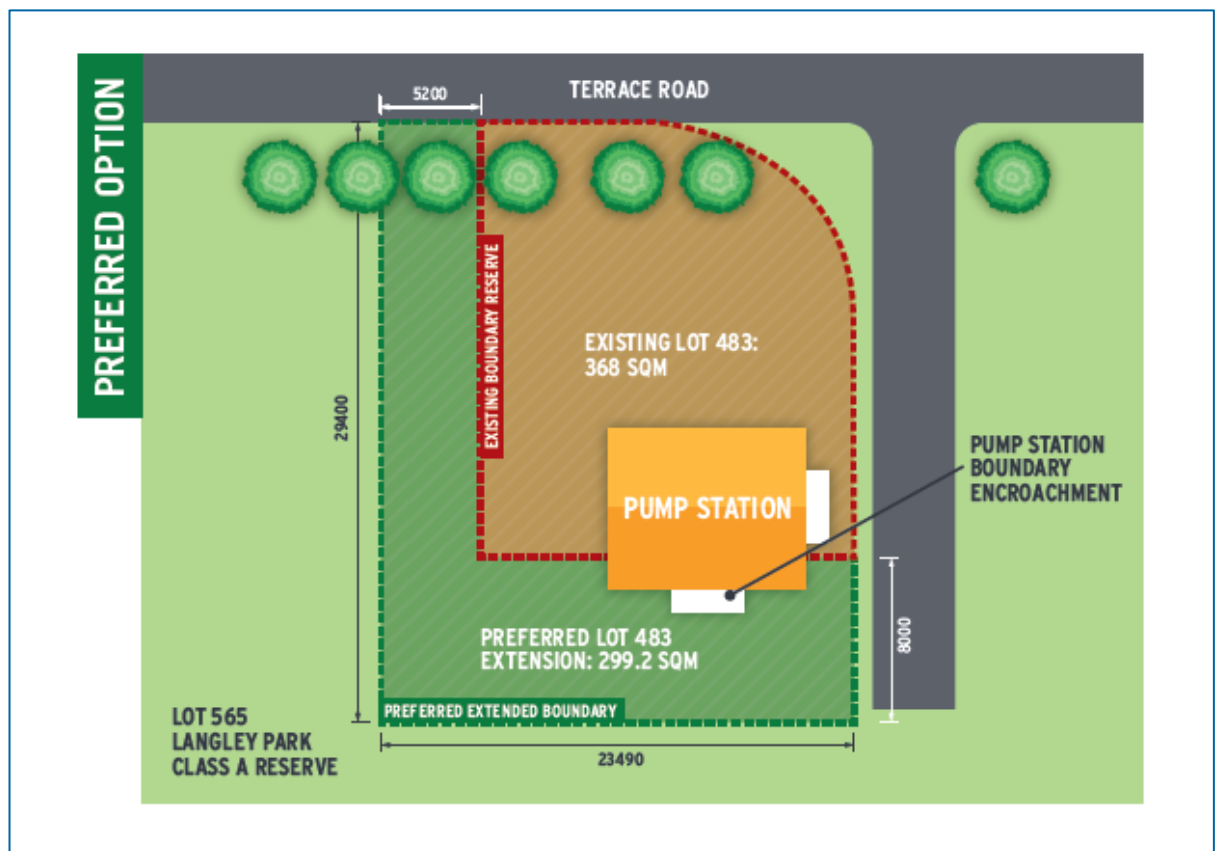


Figure 1 Nominated boundary re-alignment

(Source: City of Perth)

1.2 Consultation

The level of community interest in the future of the Langley Park pump station is best described as marginal and consistent with the level of interest in other developments advertised by the City in the locality. In the first phase of community consultation, advertising the boundary rationalisation and extension, four submissions were received. Some of the submissions took the opportunity to provide commentary on what appropriate uses might work for the Pump station. The most desired use for the Pump station was for a restaurant / café. Other suggested uses included:

- Combined museum / café
- National Trust Museum displaying early settlement history and/or aerodrome use of Langley Park
- Event/Admin Centre for TriEvents on event days
- Café/Winery

The City of Perth then undertook a second phase of community consultation regarding the adaptive reuse options for the pump station. This involved the distribution of 1200 letters and the posting of public notices both at the City of Perth offices and on the website inviting interested community members to attend a City of Perth hosted public forum to discuss potential future uses for the Pump station. A total of 13 written submissions were received and 6 people responded to the invitation to attend the public forum.

2. Public Forum

2.1 Public Forum Format

Originally, the public forum was planned to be delivered at the City of Perth's Town Hall, however due to the low number of respondents wanting to attend, the public forum was relocated to the City of Perth's office, and the delivery format adapted to suit the small number of anticipated participants. The delivery format used was a workshop.

The workshop was held on Thursday 10th September 2015 between 6pm and 8pm. Of the 6 respondents who confirmed their attendance to the workshop, a total of 4 people attended.

The workshop was delivered with the following structure:

- Introduction
- Purpose of the workshop
- Site history
- Overview of Management Order
- Planning and conservation considerations overview
- Round table discussion

A Powerpoint presentation was prepared to help guide discussion and formed the basis for the delivery of the workshop. Refer Appendix 1.

2.2 Workshop Delivery

GHD introduced the principles and purpose behind why the workshop was being undertaken. An overview of the history relating to Langley Park and the pump station, including the boundary re-alignment and the transfer of the Management Order process was provided, as a precursor

to the workshop itself. It was noted that all workshop participants had no prior knowledge of the process that had instigated further investigations into the potential future uses for the Pump station.

Previous studies and the relevant key findings were also shared with the group. Specifically, the studies referred to were:

- 2011 Structure Assessment Report
- 2011 and 2015 Commercial Feasibility Assessment
- 2010 Construction Cost Estimate
- 2006 Water Corporation Hill Street Pump station Feasibility Study for Adaptive Use

The participants were given a set of parameters to take into consideration when considering potential future use options for the pump station. These parameters were divided into conservation and planning related considerations. Specifically, these were:

| Conservation Principles | Planning Considerations |
|---|---|
| <ul style="list-style-type: none"> • All new structures are to be situated away from the heritage fabric of the building • Retain the 2 storey interior with finished trusses and visible basement of existing building • Attention being given to the setting and the relationship to the Esplanade and approach from Hill Street • That the two principle facades (south and east) of the pump station to remain intact | <ul style="list-style-type: none"> • Land use • Car parking • Bike facilities (ie parking) • Building Code of Australia requirements • Disabled Access • New structures • Vehicular access • Preferred hours of operation • Weather protection |

Table 1 : Workshop Parameters: Conservation Principles and Planning Considerations

It was also highlighted that each of the options considered and agreed to would undergo further feasibility assessments with respect to the provisions of the Building Code of Australia (BCA), the Disability and Discrimination Act (DDA) and related Australian Standards.

2.3 Workshop Outcomes

The workshop participants were briefed, through the presentation provided, as to what the purpose of the workshop was and what input was being sought from them to help inform what potential future uses could be considered for the pump station. With the number of participants being quite low, there tended to be a more focussed discussion around what uses would be considered most suitable, with a clear preference from the outset being a café.

Current issues

Initially, the participants were asked to highlight any current issues or limitations they see with the current uses and function of the pump station. Following is a list of those current issues which were highlighted in relation to the existing buildings and use of the pump station and nearby land uses:

- The change rooms to the east of the pump station need upgrading, including a baby change table for mothers to use;

- Limited options to get a coffee. Existing cafes are located on the northern side of Terrace Road, in the shade, which is not considered very hospitable;
- There is an element of anti-social behaviour occurring in the park after dark with patrons from nearby bars going to the park.
- Langley Park regularly gets inundated with water following heavy rain. Any use proposed for the pump station will need to take this into consideration.
- Something needs to be done to the pump station, as it is being left to deteriorate, and is regarded as an iconic building on the Langley Park Reserve, that needs to be kept.

Preferred uses

As previously mentioned, there was a strong stated preference among the workshop participants for a café to be accommodated in the pump station. However, in keeping with scope of the work GHD was engaged to undertake, alternative uses such as hospitality, tourism and social and community uses were also discussed. Following is a synopsis of the discussion that took place and the preferred uses. A proposed land use configuration plan has also been provided, that reflects the discussions also.

Café Use

The workshop participants (all of which were local residents living in nearby apartment buildings) commented that there was an identified lack of alternative café options within the immediate area. There was a strong consensus that a café use would be the most appropriate use for the pump station building.

The workshop discussion regarding how and in what form a café would operate included:

- hours of operation, noting that the café should predominantly be a day time use with later operating hours during summer months;
- the café would serve light refreshments and lunches, including salads, and also offer hot and cold drink options;
- the distribution of umbrellas throughout the areas surrounding the pump station building would be a 'nice touch' if provided by the café operator;
- a permanent undercover eating area could be provided to the west or south of the building, possibly replicating the same style and architecture as the existing pump station; and
- that a possible name for the café could be 'Pump Station Café'.

The participants were then asked to provide input into the land use configuration for the café use.

The workshop participants noted that given limited floor area within the pump station (78m²) to accommodate seating, the provision of outdoor seating area for café users, which included a weatherproof area, would be necessary. The key elements of how the pump station and surrounding land would be used were as follows:

- Paving adjacent to the existing vehicular access point and the curtilage of the building to allow easy access for prams and people in wheelchairs;
- Access into and out of the café would be provided by two separate access points (ie the entrance and exit points are clearly separated), utilising existing entry points into the building.
- Optimisation of the view towards the river by not allowing any solid built structure to be located on the southern portion of the site;

- Installation of public art, to activate the area located to the north of the pump station
- Provision of bicycle racks adjacent to the change rooms; and
- The provision of outdoor furniture along the western boundary of the site, creating a connection with the greater Langley Park precinct and optimising its vantage point towards Elizabeth Quay.

There were two options developed with workshop participants, which have been presented in Figures 2 and 3 overleaf.



Figure 2: Café Use Option 1

Café Use Option 1

- A primary (weather proof) outdoor dining area located to the west of the pump station building, with a secondary outdoor dining area (alfresco) located to the south-west.
- Hardstand access, using brick paving or cobblestones (not bitumen) provided within the immediate curtilage of the building, primarily to allow easy access for wheelchairs and prams.
- Single entry and exit points into cafe
- Street furniture / Umbrellas along the western side of the pump station
- Public Art installation in the northern portion of the land parcel.



Figure 3: Café Use Option 2

Café Use Option 2

- Weatherproof outdoor dining area located in the south-east corner of the site
- Alfresco outdoor dining and seating to the south and west
- Hardstand access, using brick paving or cobblestones (not bitumen) provided within the immediate curtilage of the building, primarily to allow easy access for wheelchairs and prams.
- Single entry and exit points into cafe
- Street furniture / Umbrellas along the western side of the pump station
- Activation of northern portion of site with public art installation.

Museum Use

A museum use was put forward as a suggested use for the participants to consider. While there was a distinct lack of preference for this use, there was discussion around how this use might work if it were to be considered as a future use for the pump station. To prompt discussion, reference was made to the conceptual plans developed by Hocking Architects for the Water Corporation in 2006 which were referred to in the workshop presentation. Refer Figure 4 below.

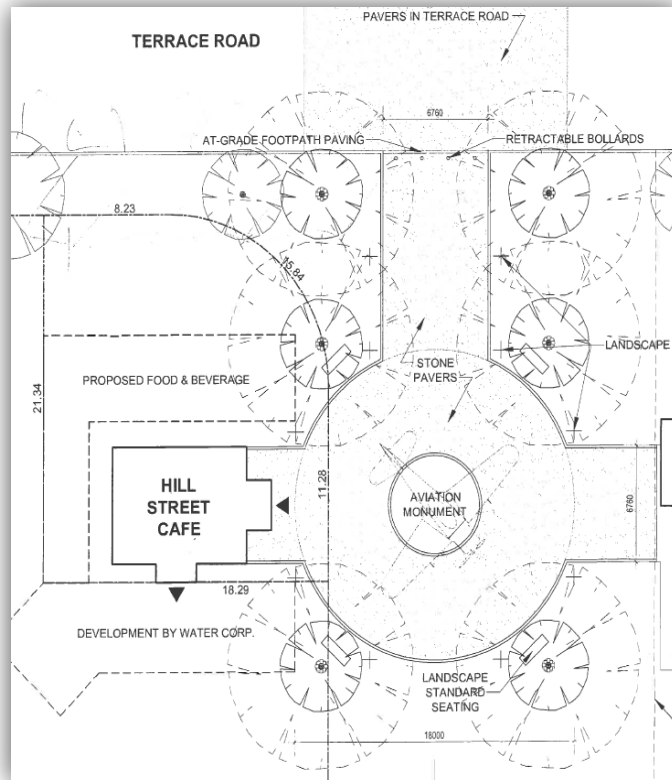


Figure 4: Hocking Planning and Architecture Conceptual (2006)

Overall, the participants believed that a museum use on its own would not be a viable option, and without the museum being operated by the National Trust, it would likely be a one-time destination for tourists and visitors to the area. The synergies with nearby tourist attractions such as the Perth Mint, St George's Cathedral and St Mary's Cathedral, were noted, nonetheless, the distance from these locations to the pump station was considered to be too significant, and a deterrent for people visiting the pump station museum.

A suggested option was to combine both a museum function of the building with a café, which would then offer multiple uses, and still address what the workshop participants identified as a primary need for this area (ie a café).

A Museum could comprise a combined display that paid homage to the previous use of the building, namely as a pump station, and also to the informal aerodrome use of the greater Langley Park. The installation of public art work like that shown in the Hocking Plan (a replica plane) and the offering an interactive public art display for children to discover and learn about the history of flying, for example, was positively discussed. A schematic showing the proposed land use configuration is provided below.



Figure5: Proposed Museum/Café Option

Key elements of a proposed Museum/Café use would comprise:

- Hardstand access, using brick paving or cobblestones, but not bitumen is provided within the immediate curtilage of the building, primarily to allow easy access for wheelchairs and prams
- Single entry and exit points into cafe
- Installation of interactive public art that ties in the with themes of the pump station café (eg a 'trip around the world' art installation that mimics the Smith brothers England to Australia air race in 1923)
- Provision of public barbecues in the northern portion of the site, with possibly an additional public art installation in this location to add visual interest.
- Additional public barbecues to the east of the site, to create a connection with the greater precinct area.

Other Uses

A number of other uses were suggested and participants invited to discuss further, however, there was very little interest in wanting to explore these alternative options further. Specifically, those suggested and the related responses were:

- Shop - the area of the building was too small to offer any space for a shop to operate and offer things to sell
- Wine centre – this would not work in this location
- Tourism attraction (other than those already discussed) – the tourist destination for Perth is Forrest Place and it should stay this way.
- Gym - gyms and fitness centres exist everywhere, including all nearby apartments. Introducing a gym would be oversupplying an already underutilised facility.
- Events staging store – this would not activate the space, which is badly needed.

Suggested uses for the Family Friendly Precinct

From the outset, an interesting observation was that the workshop participants associated the pump station as a part of the wider precinct area that includes the existing change rooms and playground, situated east of the subject site. As a result, the discussions which related to possible uses for the Pump station tended to be influenced by how the pump station use would relate to the remainder of what was identified as a “family friendly precinct”.

The feedback received was that there should be consideration of the wider precinct that includes the playground and change rooms, located east of the existing pump station. A depiction of what this ‘Family Friendly Precinct’ might comprise is provided overleaf in Figure 6

Suggested additional facilities that could be introduced included:

Horizontal Climbing Wall

Installing a horizontal climbing wall, such as that provided opposite that is affixed to the back of the change rooms for children to play on, and to also connect the existing playground with the pump station.



Public Barbecues

Placing public barbecues, like those found in City Beach and Kings Park, for the public and users of Langley Park to use on weekends.

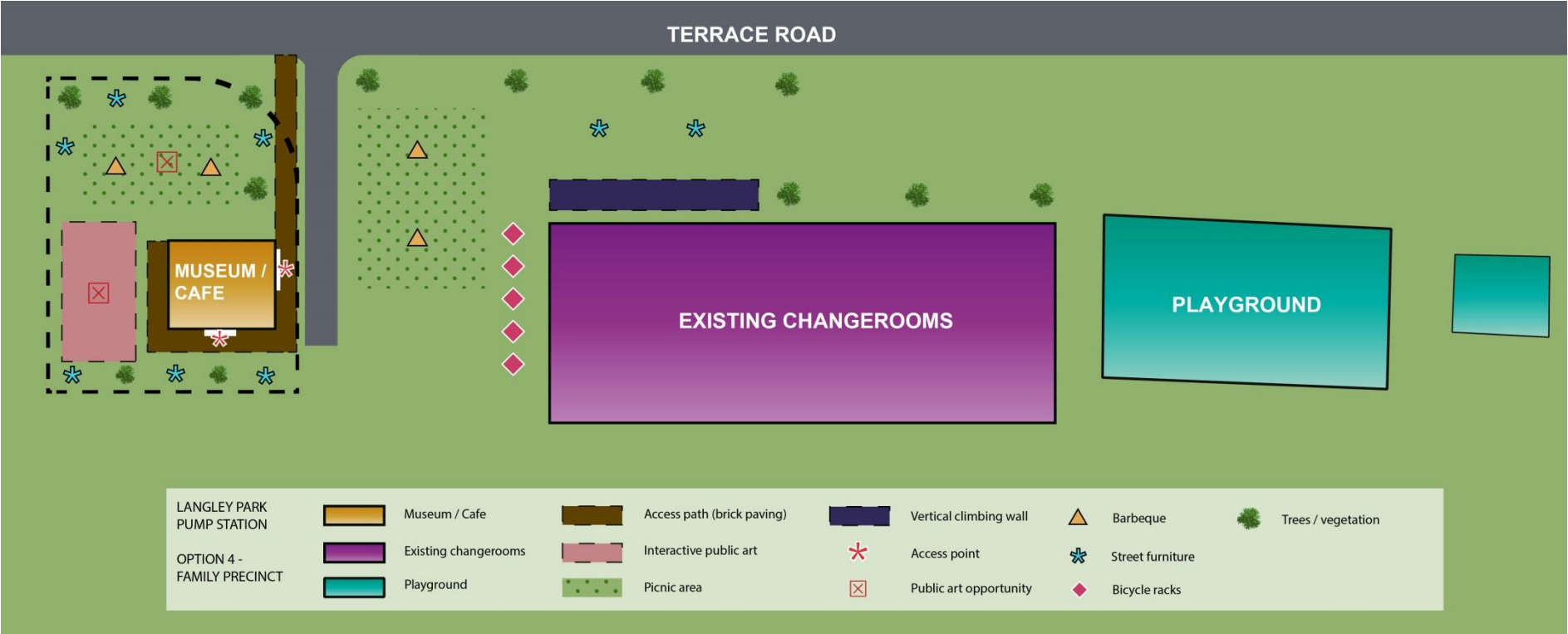


Figure 6: Family Friendly Precinct

3. Development and Management Issues

Any future use of the pump station will result in an intensification of activity in this location, regardless of what preferred use the City of Perth decides to proceed with. There will need to be appropriate management responses put in place to ensure that any development and use in this location meets the relevant legislative requirements.

3.1 Built Structure

The pump station consists of three levels; pump well, ground level and mezzanine level. The building is a brick construction with an exterior limestone façade and tiled roof. The roof consists of timber battens spanning between trusses.

The interior of the building has tiles to approximately half height of some walls and the area around the pump well has a concrete floor at the entrance to the building and checker plate flooring in other locations. Any improvements to the building will need to be cognisant of the heritage status of the pump station. Any internal refurbishment to the building should acknowledge the original use of the building as a pump station. For example, when upgrading the floor, possibly consider the installation of suspended Perspex floor which shows the original pump station infrastructure.

To make the building useable for a possible café use, the entry located on the eastern façade would need to be re-opened, in order to allow access into the building. Any proposed use for the building would need to ensure that the access points on both the eastern and southern facades are reinstated, and that each be nominated as either a entry or exit point only.

All upgrades to the building will be required to comply with the Building Code of Australia provisions.



Source: GHD Photo

3.2 Heritage Considerations

The pump station, identified as Pump station No.2 was entered into the State Register of Heritage Places in 2004. At this time it was identified that, despite being unused for 15 years, the building remained in relatively good condition. This condition has deteriorated significantly since.

While there is some deterioration identified, the majority of the building has been assessed to be in good structural condition. It has been recommended in a Structural Assessment Report prepared in 2011 that some works will be required to both the internal and external elements of the structure, to rectify deterioration and to minimise any ongoing deterioration.

The report listed the following ongoing maintenance requirements being carried out for the pump station in relation to the following, in order to reduce the rate of deterioration:

- Brick walls
- Exterior timber
- Monorail support beam
- Foundations
- Roof structure

Any works that will be undertaken will need to ensure that appropriate measures are put in place to protect the heritage fabric of the building. This will include the siting of any new structures away from the heritage fabric, and that any structures that may be built in this location are respectful of the existing heritage building.



3.3 Planning Considerations

3.3.1 Change of Use

The pump station is located in an A class reserve and is zoned Parks and Recreation under the Metropolitan Region Scheme (MRS). Subject to the conditions of the Management Order, there may be the option for the City of Perth to be allocated the 'power to lease' this land for future uses, as considered appropriate by the Western Australian Planning Commission (WAPC). A change of use application pursuant to the MRS would need to be submitted to the WAPC by the City Perth for any future use of the building (pump station) that is not related to public works and any developments associated with the related use.

Any additional structures and related uses would also require the approval of the WAPC for the introduced use, and would likely be referred to the City of Perth for building approval.

3.3.2 Car Parking

Land uses usually have a car parking requirement assigned to them, which is calculated on floor area. In this instance, it would be anticipated that the car parking requirements for this use would be relaxed, given the requirement to provide on-site car parking would not be seen as preferable for the greater Langley Park reserve and the Metropolitan Region Scheme does not specify parking requirements. There is paid street parking within close proximity along Terrace Road as well as nearby public car parks, and public transport services running along Terrace Road, which further alleviates demand for onsite parking.

It is noted that the Langley Park Precinct Plan does allow for small well-landscaped carpark development on private land, and this should be considered when determining the most appropriate response to addressing car parking requirements for any proposed use.

3.3.3 Disabled Access

The change of use and any change to the building fabric to accommodate a new use would need to ensure that the proposal meets the requirements of the Disability Discrimination Act 2009. Noting the heritage status of the building itself, there may be a requirement to consider alternative options for access to the building, that may require some level of dispensation being given by the Heritage Council to ensure universal access is provided.

The provision of disabled access car parking will also need to be taken into consideration for any future use of the proposed building.

3.3.4 Amenities

The current building was not constructed to be adapted for another use, and as such, only offers limited facilities, as far as amenities. The pump station has male only amenities provided within the existing building. There are no female ablutions provided within this building.

For any future use of the building, it is recommended that the City consider utilising the nearby change room toilets as a shared facility between both the change rooms and the future pump station use. This would be subject to Council approval.

3.3.5 Additional Structures

The options proposed within this report all suggest the introduction of non-intrusive structures within close proximity to the existing pump station building. This is considered an appropriate and necessary response for a use such as a café, given the small floor area of the pump station (78m²) and the restrictions on doing any substantial work, aside from improvements to the existing built form given the heritage status of the building.

The structures that have been nominally considered are proposed to be a combination of an open and partially enclosed structure. The enclosed component of the structure should be glazed, so to ensure visual permeability to the river, from all aspects within the pump station subject site.

4. Summary

There is a clear indication that there is a community desire to see further activation of the Langley Park Reserve, and that the introduction of a new use to be housed in the existing pump station is strongly supported by the community.

The workshop confirmed that the preferred future use for the Pump station is for a café. The basis of this use being seen as the most suitable use is that there is an identified deficiency of café options that exist away from the main CBD thoroughfare, which is Adelaide Terrace, in this location.

The introduction of a café use is regarded as a compatible use that will complement and enhance the existing uses already occurring on Langley Park. It is highly probable that the café will be well attended, given the significant number of residential premises within the immediate locality.

This report: has been prepared by GHD for City of Perth and may only be used and relied on by City of Perth for the purpose agreed between GHD and the City of Perth as set out in the Project Brief.

GHD otherwise disclaims responsibility to any person other than City of Perth arising in connection with this report. GHD also excludes implied warranties and conditions, to the extent legally permissible.

The services undertaken by GHD in connection with preparing this report were limited to those specifically detailed in the report and are subject to the scope limitations set out in the report.

The opinions, conclusions and any recommendations in this report are based on conditions encountered and information reviewed at the date of preparation of the report. GHD has no responsibility or obligation to update this report to account for events or changes occurring subsequent to the date that the report was prepared.

The opinions, conclusions and any recommendations in this report are based on assumptions made by GHD described in this report. GHD disclaims liability arising from any of the assumptions being incorrect.

GHD has prepared this report on the basis of information provided by City of Perth and others who provided information to GHD (including Government authorities), which GHD has not independently verified or checked beyond the agreed scope of work. GHD does not accept liability in connection with such unverified information, including errors and omissions in the report which were caused by errors or omissions in that information.

Appendices

Appendix A – Presentation



Langley Park Water Pump Station

Adaptive Reuse Options



PURPOSE



Langley Park Water Pump Station

Exploring Alternative Uses for the Adaptive Reuse



OVERVIEW



YESTERDAY, TODAY, TOMORROW

"In my garden, there is a large place of sentiment. My garden of flowers is also my garden of thoughts and dreams."



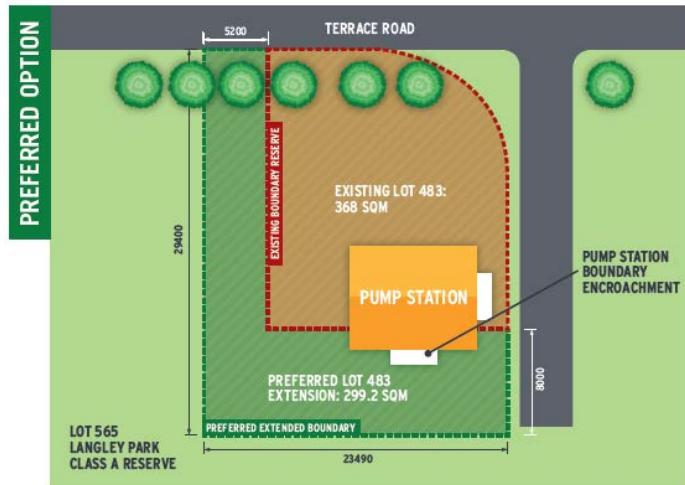
MANAGEMENT ORDER



City of Perth was approached by Water Corporation to take over Management Order, under *Land Administration Act 1997*

MANAGEMENT ORDER TRANSFER

Boundary Re-alignment and Rationalisation

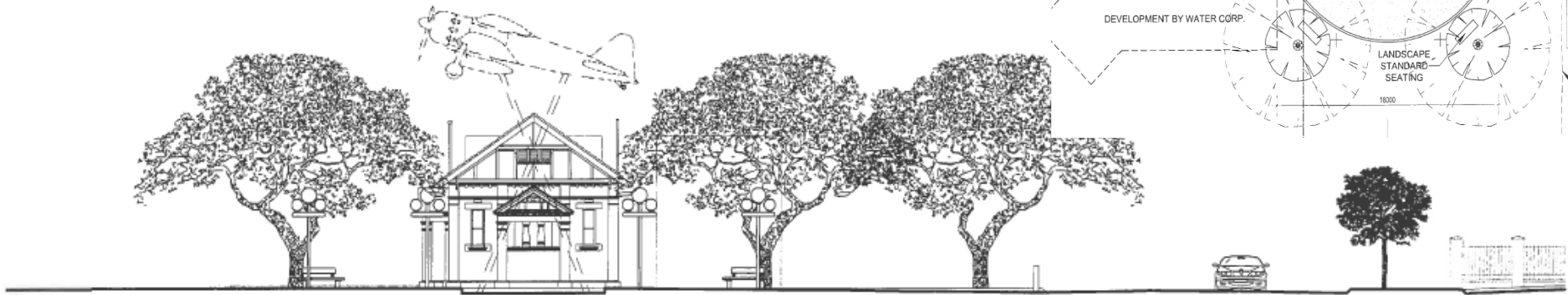
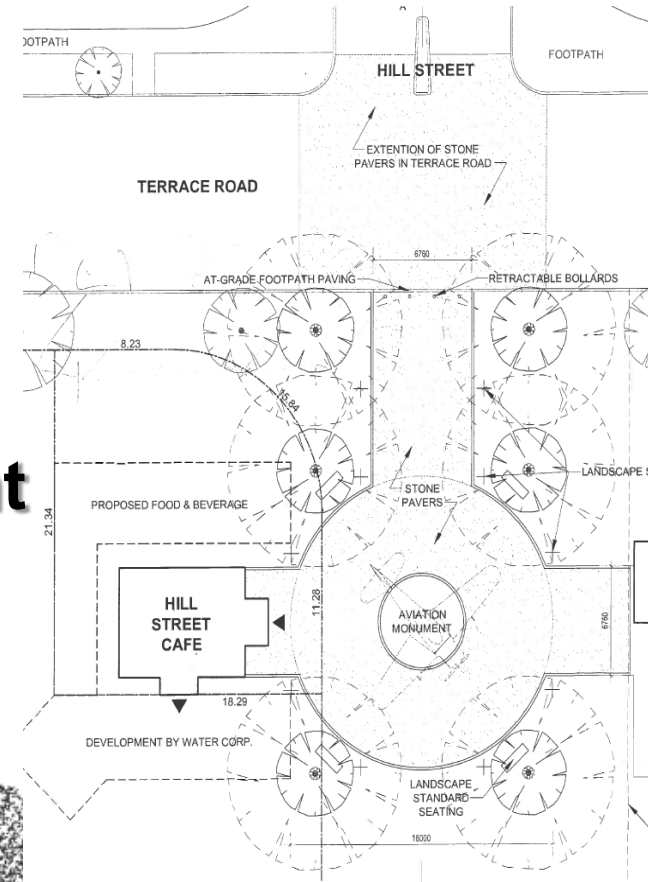


Potential Alternative Uses for Pump Station



Previous Investigations

- **Structural**
- **Construction Cost Estimates**
- **Economic Activation Assessment**
- **Potential Reuse options**



LANGLEY PARK

HILL STREET ESPLANADE THRESHOLD PARK
(AVIATION MONUMENT)

TERRACE ROAD

HILL STREET

PLANNING CONSIDERATIONS

Planning Considerations

1. Land Use Permissibility
2. Context
3. Car Parking
4. Bicycle Parking
5. Toilets
6. Tenancy
7. Disabled Access
8. Vehicular Access
9. New Structures
10. Weather Protection
11. Hours of Operation



PLANNING CONSIDERATIONS

| Planning Scheme / Policy | Relevant to the subject site |
|--------------------------|------------------------------|
| Region Scheme : | MRS |
| Region Scheme Zoning: | Parks and Recreation |
| Local Government Area: | City of Perth |
| Planning Scheme: | Town Planning Scheme No.2 |
| Gazettal Date: | 9 January 2004 |
| City of Perth Precinct: | P12 – Langley Park |
| Zoning: | Parks & Recreation |

CONSERVATION PRINCIPLES

Conservation Principles

1. All new structures away from heritage fabric
2. Retain 2 storey interior with finished trusses and visible basement
3. Attention to setting and relationship to Esplanade and approach from Hill Street
4. Two principle facades (south and west)



Our goal is to determine potential future uses for the waste pump station





www.ghd.com

Appendix B – Workshop Notes

CAFÉ/FAMILY PRECINCT

Langley Park Pump Station

Existing pump station

- Kitchen/ordering
- 2 access points.
(BCA)

Outdoor Dining Area

- weatherproof (partial)

Barbecue Area

- may be behind changerooms
- next to vehicular.

Family Precinct

Museum element

Focus on waterpump/airfield.

Upgrades to:

- Change rooms (to incl. Mother's room)/toilets.
- Vehicular access
bitumised,
+ roundabout (for drop-offs)

Considerations

- employee carparking
- delivery trucks.
(loading zone)
- landscaping (more please).
natural shading.



LEGEND



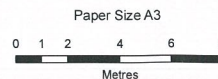
Useable Area
(667.2m²)



Vehicle Access Point
(188m²)



Tree / Existing
Vegetation





MUSEUM

Langley Park Pump Station

Integrated with cafe.

Public Art

What for children?

- interactive playground
- horizontal climbing wall.
- ↳ • (change rooms).

Fact sheet / Scavenger hunt
through Public Art. (interpretive)
(if Museum open)

BBQ's

Park benches

Museum has limited use
(its a once a year exp.)

Langley Park museum

Lighting locs

LEGEND



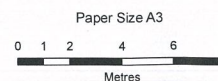
Useable Area
(667.2m²)



Vehicle Access Point
(188m²)



Tree / Existing
Vegetation



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| 1 | Kylie Harrison | Colleen Thompson |  | Colleen Thompson |  | 24.09.15 |
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Planning Committee
Confidential Schedule 4
(Minute PL230/15 refers)

Distributed to Elected Members under separate cover

Bound in Consolidated Committee
Confidential Minute Book
Volume 1 2015



City of Perth
Langley Park Pump Station
Structural Condition Audit

September 2015

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Appendices

Appendix A – Floor Plan

Appendix B – Photographs

Appendix C – Table C1 – AS2870

1. Introduction

GHD was engaged to conduct a structural inspection and asbestos containing materials survey of the heritage listed pump station No. 2, Langley Park. The inspection was requested as an updated to the October 2011 report that was commissioned to assist the City of Perth to complete the Economic Place Activation Assessment for the Pump Station. The pump station is a heritage listed building and any works to the building will need to maintain the heritage of the building. This report will address the structural concerns of the building and make recommendations for remediation works where necessary to upgrade the building for safe use. It will also consider any maintenance or repairs that will be required to maintain the integrity of the building in the short term.

The inspection consisted of a visual assessment only of the building. Only areas where safe access was available have been inspected. The pump well was not inspected as this was full of water; also the mezzanine level was not accessed.

1.1 Limitations

This Langley Park Pump Station ("Report"):

1. has been prepared by GHD Pty Ltd ("GHD") for City of Perth;
2. may only be used and relied on by City of Perth;
3. must not be copied to, used by, or relied on by any person other than City of Perth without the prior written consent of GHD;
4. may only be used for the purpose of determining modifications required to gain safe access to the pump station (and must not be used for any other purpose).

GHD and its servants, employees and officers otherwise expressly disclaim responsibility to any person other than City of Perth arising from or in connection with this Report.

To the maximum extent permitted by law, all implied warranties and conditions in relation to the services provided by GHD and the Report are excluded unless they are expressly stated to apply in this Report.

The services undertaken by GHD in connection with preparing this Report:

- were limited to those specifically detailed in section 1 of this Report;
- did not include inspecting those areas not visible or provided with safe access, or any tests or measurements of the structure.

The opinions, conclusions and any recommendations in this Report are based on assumptions made by GHD when undertaking services and preparing the Report ("Assumptions"), including (but not limited to):

- the façade of the building is not hiding any significant defects to the building structure
- Footings and other areas not visible or accessible are structurally sound
- Connections are structurally sound

GHD expressly disclaims responsibility for any error in, or omission from, this Report arising from or in connection with any of the Assumptions being incorrect.

Subject to the paragraphs in this section of the Report, the opinions, conclusions and any recommendations in this Report are based on conditions encountered and information reviewed at the time of preparation and may be relied on until March 2012, after which time, GHD

expressly disclaims responsibility for any error in, or omission from, this Report arising from or in connection with those opinions, conclusions and any recommendations.

2. Background Information

The Pump station is located in Langley Park near Hill Street. Construction of the pump station was completed in February 1914. It operated as a waste water pumping station and amenities block until it was decommissioned in 1989. The original entrance to the amenities building was filled in 1992 and there is no record of significant maintenance occurring to the building since this time.

The pump station consists of three levels; pump well, ground level and mezzanine level. The building is a brick construction with an exterior limestone façade and tiled roof. The roof consists of timber battens spanning between trusses. The main pump station ceiling structure comprises of boards spanning onto timber trusses. The interior of the building has tiles to approximately half height of some walls. The area around the pump well has a concrete floor at the entrance to the building and checker plate flooring in other locations. Refer to the sketched floor plan in Appendix A for more details.

The building has been heritage listed and as such any modifications will require approval from the heritage council. The October 2011 report identified some repairs and further works that would be required before the building could be restored for use. The report also recommended some general ongoing maintenance to prevent deterioration of the building.

3. Structural Inspection

The structural inspection was completed on 18 August 2015. Due to the presence of water in the pump well the inspection was concentrated on the structure above ground level.

Overall the building appears to be in reasonable condition, however it has deteriorated since the previous inspection in 2011 and some repairs are required to prevent further deterioration.

3.1 External Inspection

The exterior of the building has a painted limestone/sandstone façade. There is some evidence of deterioration to the exterior of the building. There is damage to some sheeting panels of the gable end wall; however this is not of a structural nature and can be easily repaired or replaced. Photograph 1 and Photograph 2 show the deterioration that has occurred between 2011 and 2015.

The timber head beam on the western gable end of the building is also showing signs of deterioration. Photograph 3 and Photograph 4 show the deterioration that has occurred between 2011 and 2015. The waterproofing of the end gable, particularly on the western side of the building has been compromised. Photograph 4 shows the deterioration between the fascia beam and the roof tiles. The fascia beam has also deteriorated and requires repair or replacement as it has deteriorated through lack of painting or protection from the weather.

The pavement to the South East corner of the building has cracked and subsided. This is causing a trip hazard. While it is not structural in nature, it is a potential safety hazard that should be rectified.

Repairs and/or maintenance do not appear to have been undertaken since the 2011 report. The building has further deteriorated without any maintenance work.

3.2 Internal Inspection

An inspection of the interior of the building revealed some cracking to the building, however generally the building appears to be in reasonable condition. The cracks observed inside the building do not appear to have changed significantly since the 2011 inspection.

Cracks in the walls appear to be generally 2mm wide, which equates to damage category 2 according to Table C1 of AS2870 (attached in appendix C). Cracks of this size can be easily filled and are not a structural concern. However, these cracks should be monitored on a regular basis to make sure further cracking/movement is not occurring.

There are also cracks propagating from the monorail support beam. These cracks have most likely occurred at this location due to disruption to the continuity of the wall. However, if the monorail is intended to be recommissioned vibration could increase this damage. Therefore, further investigation into these cracks and a possible revised safe working load will be required. If the monorail is not intended to be recommissioned these cracks can be treated and monitored as general cracks to the building.

The platform around the well pit on the northern side of the building has corroded and is no longer safe to access. The steelwork for this platform was not visible during the 2011 inspection due to water level in the pit. However, it is assumed based on its current state that the steelwork has further corroded, under repeated wetting and drying cycles. Photograph 7

The mezzanine level above the old ablutions block was inaccessible and as such was not inspected as part of this work. If the mezzanine area is to be utilized, further investigation will be required to confirm the structural integrity of the area. Safe access will also need to be

provided. Previous documents indicate that there were originally stairs providing access to this area. Based on general deterioration of the building and observations of water ingress, it is assumed that the roof over the mezzanine area will also require waterproofing.

Non-structural deterioration has also occurred to the building, most likely due to water ingress. The timber ceiling boards still appear to be in good condition, however the fixings are coming loose. The cause of the loose boards is most likely a combination of forces due to shrink and swell in the timber under repeated wetting and drying and deterioration of the fixings.

Photograph 5 is a photo of the roof/ceiling structure in 2011, while Photograph 6 shows the deterioration in 2015.

The window frames have deteriorated and in some instances have fallen off the wall. One section of the internal window frame was observed floating in the water in the well. These items should be restored and the waterproofing rectified to maintain the building. Photograph 10

Water damage and debris was observed in the building to approximately 50 – 100 mm above finished floor level. Photograph 8 This is evidence of the fluctuation in the ground water level in this area. If this building is to be used on a regular basis, the ingress of groundwater into the building would have to be prevented. This would also require preventing water from entering from the outside of the building.

4. Recommendations and modifications required

The following recommendations are divided into two categories; short-term repair and maintenance requirements and future use requirements. The short-term repair and maintenance items are required to maintain the integrity and value of the building. Without completing the short-term repairs the building will continue to degrade and require significant repairs to restore the building and maintain the heritage of the building. The future use requirements are those items required to make the building safe for future use, however will not significantly impact further deterioration of the building's current state.

4.1 Short Term Repair and Maintenance Requirements

- Waterproofing of the roof
- Repair of fascia beam and roof tiles to prevent further water ingress
- Paint exposed timber to protect from deterioration;
- Refixing loose ceiling boards
- Barricade the well platforms to the north side to prevent access onto the existing platform.
- Fix trip hazard by filling at the south east corner of the building

4.2 Future Use Repairs and Maintenance:

- Possible filling in or covering of the well and valve pit to prevent a safety hazard. If this is not to be filled in or covered, safe access will need to be provided.
- Repair or replacement of the existing platforms around the wells.
- Addition of safe access, for example stairs, to the mezzanine area if it is to be utilized. The mezzanine area will also require a full inspection prior to determining its future use to ensure suitable capacity. If it is to be used for anything other than storage an investigation into the ventilation may be required.
- If the monorail is to be recommissioned a detailed inspection of the connection with the wall and monorail itself may be required. The monorail may need a revised safe working load applied to it.
- Repair of the head beam on the western gable end of the building. This also includes other minor external timber members that have been damaged by the weather.
- Repair to the gable end wall to replace the damaged panels. This is not of a structural nature but is required to maintain a similar façade.
- Aesthetic repair of cracks could be achieved by repointing the brickwork around the cracks.

4.3 Further investigations and repairs

Further investigations may be required before the pump station can be used. The items below were noted during the inspection. This list is not comprehensive or within the structural engineers experience area, therefore these items will be conducted by others.

- Investigation into the water seepage into the well to determine the source of the water and the required repair method. A structural inspection of the well walls could be conducted simultaneously provided it is safe to do so.
- If the ablutions area is to be recommissioned the existing pipework etc. will have to be investigated for suitability for future use.
- Ventilation assessment for the mezzanine floor may be required if using it for purposes other than storage.
- Electrical Conduits appear to be decommissioned. Some electrical cables/conduits are submerged in the well. Therefore an electrician will be required to rewire the building before power can be supplied to the pump station.

GHD can provide services to investigate the water seepage and complete a ventilation assessment if required.

5. Ongoing Maintenance Required

Ongoing maintenance of the building will be required to ensure it remains in good condition. The maintenance items listed below are typical for a building of this age and construction and no specific maintenance is required.

5.1 Brick Walls

Ongoing monitoring of the brick walls will be required to ensure that the existing cracks in the building do not open further and become a structural problem.

5.2 Exterior Timber

The timber to the exterior of the building will require regular painting to protect it from further degradation caused by exposure to the weather.

5.3 Monorail Support Beam

The monorail support beam will require regular inspections to ensure that further cracking to the brickwork around the beam has not occurred. However, if the monorail is not recommissioned these cracks can be treated similarly to the other cracks and monitored as part of the monitoring of the brickwork walls.

5.4 Foundations

The foundations should be monitored to ensure that further degradation does not occur.

5.5 Roof structure

The roof will require regular maintenance to ensure that it remains in good condition and that no leaks occur. This is standard maintenance for a building and should not require assistance from a structural engineer.

Appendices

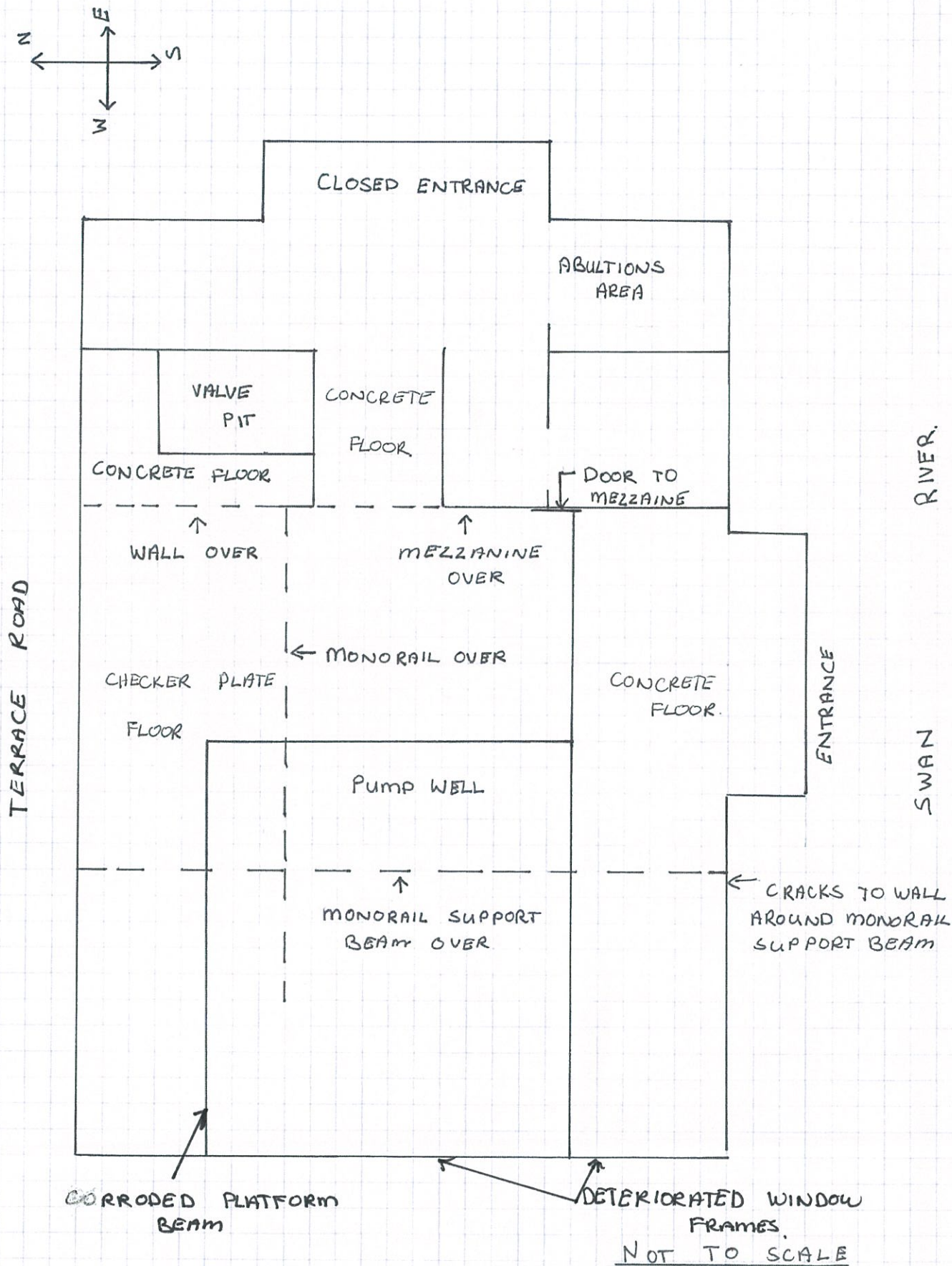
Appendix A – Floor Plan



Client CITY OF PERTH
Project LANGLEY PARK PUMP STATION
Subject FLOOR PLAN

Job no. 61132537
Calcs by C.M.H.
Checked by

Sheet 1 of 1
Date
Date



Appendix B – Photographs



Photograph 1 Western End of Pump Station –
September 2011



Photograph 2 South and Western Elevation -
August 2015



Photograph 3 Western End Head Beam –
September 2011



Photograph 4 Western End Head Beam/Facia -
August 2015



Photograph 5 Roof Structure over Pump Well -
September 2011



Photograph 6 Roof Structure over Pump Well -
August 2015



Photograph 7 Platform on Northern side of Well



Photograph 8 Water Damage around base of wall



Photograph 9 Damage to Pavement on South Eastern corner of building



Photograph 10 Deterioration of Window Frame



Photograph 11 Deterioration of exterior Window Frame Western End



Photograph 12 Typical Cracks to interior walls

Appendix C – Table C1 – AS2870

APPENDIX C

CLASSIFICATION OF DAMAGE DUE TO FOUNDATION MOVEMENTS

(Normative)

Classification of damage with reference to wall is given in Table C1. Classification of damage with reference to concrete floors is given in Table C2.

TABLE C1

CLASSIFICATION OF DAMAGE WITH REFERENCE TO WALLS

| Description of typical damage and required repair | Approximate crack width limit (see Note 1) | Damage category |
|--|--|------------------|
| Hairline cracks | <0.1 mm | 0 Negligible |
| Fine cracks that do not need repair | <1 mm | 1 Very slight |
| Cracks noticeable but easily filled. Doors and windows stick slightly | <5 mm | 2 Slight |
| Cracks can be repaired and possibly a small amount of wall will need to be replaced. Doors and windows stick. Service pipes can fracture. Weather tightness often impaired | 5 mm to 15 mm (or a number of cracks 3 mm or more in one group) | 3 Moderate |
| Extensive repair work involving breaking out and replacing sections of walls, especially over doors and windows. Window frames and door frames distort. Walls lean or bulge noticeably, some loss of bearing in beams. Service pipes disrupted | 15 mm to 25 mm but also depends on number of cracks | 4 Severe |

NOTES:

- Where the cracking occurs in easily repaired plasterboard or similar clad-framed partitions, the crack width limits may be increased by 50% for each damage category.
- Crack width is the main factor by which damage to walls is categorized. The width may be supplemented by other factors, including serviceability, in assessing category of damage.
- In assessing the degree of damage, account shall be taken of the location in the building or structure where it occurs, and also of the function of the building or structure.

GHD

GHD, 999 Hay Street, Perth, WA 6000

P.O. Box 3106, Perth WA 6832

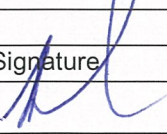
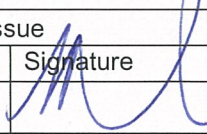
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Document Status

| Rev No. | Author | Reviewer | | Approved for Issue | | |
|---------|-----------|----------|---|--------------------|---|----------|
| | | Name | Signature | Name | Signature | Date |
| 1 | C Hermann | P Tonkin |  | P Tonkin |  | 29/09/15 |
| | | | | | | |
| | | | | | | |

SCHEDULE 6

Map and Photographs



Planning Committee
Confidential Schedule 7
(Minute PL231/15 refers)

Distributed to Elected Members under separate cover

Bound in Consolidated Committee
Confidential Minute Book
Volume 1 2015

AMENDMENTS

1. Illustrate and renumber a section of H504 to H877
2. Deproclaim a section of H504

NOTES:

Roads and/or paths are represented by centrelines which are a notional reference line that generates and approximates the centre of the pavement extent.

Ramp intersections are shown schematically. The point at which the lines meet represents the end of the median which separates the converging or diverging carriageways.

Slip lanes where ramps intersect other roads are not shown.

PROCLAMATION LEGEND

EXISTING PROCLAIMED ROUTE

H6 Highway
(^ Road Name)

M42 Main Road
(^ Road Name)

SECTION TO BE DE-PROCLAIMED

~~H2~~ ~~M12~~

SECTION TO BE PROCLAIMED

H6 Highway

M42 Main Road
(^ Landgate approved local usage name)

P18 Main Roads Controlled Shared Path

Shared paths depicted on this drawing are those for which Main Roads Western Australia has responsibility.

Shared paths the responsibility of other entities are not shown.

Z794 Public Transport Authority bus ramp

This is to certify that Council endorses the proposal shown on this plan.

Council Resolution: 297/12 of

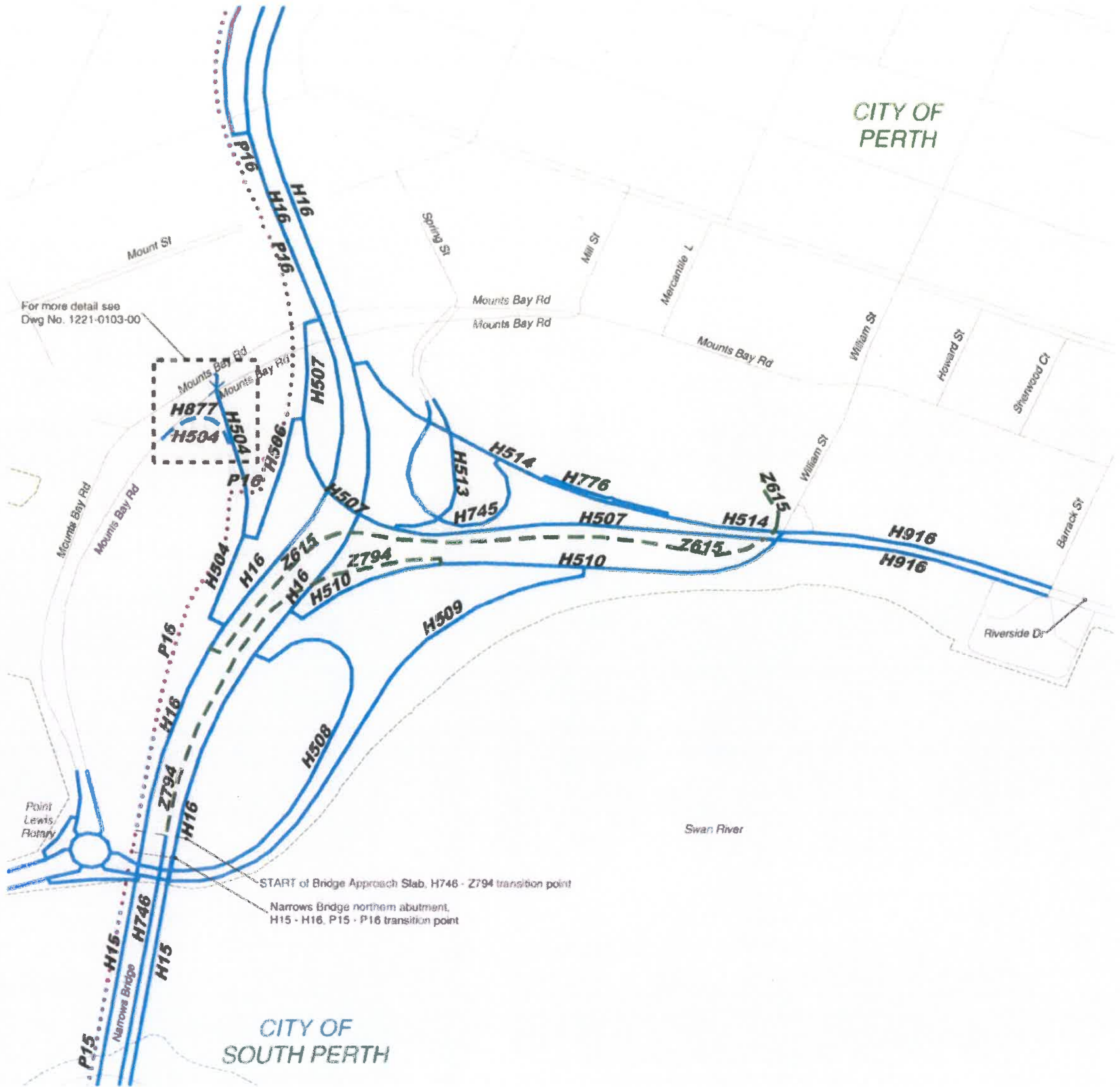
Council Meeting Held On: 26 June 2012

Chief Executive Officer
Bonnie

Date: 29/8/2012

Gazette: 23 November 2012

Page No: 5717, 5718



CITY OF PERTH

BASE MAP LEGEND

- Local Government Boundary
- Town Site Area And Boundary
- Cadastre
- Local Road

NOTE: FOR DETAIL OF TOWNSITES - REFER TO TOWNSITE DRAWINGS

| | | | | | |
|--|---|---|--|-------------------------------------|------------------|
| DATE OF ORIGINAL ISSUE: 23 Dec 2009 | 0 50 100 200 300 400 Metres | PROCLAMATION PLAN Declared Roads Narrows Interchange Proclamations | | MAIN ROADS WESTERN AUSTRALIA | |
| DRAWING AMENDMENT DATE: 27 Apr 2012 | | CITY OF PERTH - LG No. 124 | | DWG TYPE: 20.00 | FILE No: 03/4585 |
| RECOMMENDED: <i>Rampart map 03/05/2012</i> | APPROVED: <i>Laurel Morgan EDPs 15/12</i> | Planning and Technical Services Directorate | | DWG NUMBER: 0921-0188-02 | |
| SHEET 2 of 5 | | | | | |

NOTES:

Roads and/or paths are represented by centrelines which are a notional reference line that generates and approximates the centre of the pavement extent.

Ramp intersections are shown schematically. The point at which the lines meet represents the end of the median which separates the converging or diverging carriageways.

Slip lanes where ramps intersect other roads are not shown.

AMENDMENTS

- 1. Proclaim sections of H508, H509 and H510
- 2. Deproclaim H916 (a section of Riverside Dr - Elizabeth Quay)
- 3. Deproclaim sections of H509, H510
- 4. Renumber a section of H508 to H509
- 5. Renumber PTA roads in the Narrows Interchange, Z615 - 124Z028, Z794 - 124Z029.
- 6. Proclaim PTA roads in the Narrows Interchange, 124Z028, 124Z029

PROCLAMATION LEGEND

EXISTING PROCLAIMED ROUTE

- H6** Highway
(* Road Name)
- M42** Main Road
(* Road Name)

SECTION TO BE DE-PROCLAIMED

- ~~H2~~
- ~~M12~~

SECTION TO BE PROCLAIMED

- H6** Highway
- M42** Main Road

*Landgate approved local usage name

- P18** Main Roads Controlled Shared Path

Shared paths depicted on this drawing are those for which Main Roads Western Australia has responsibility

Shared paths the responsibility of other entities are not shown.

- 124Z028** Public Transport Authority bus ramp

This is to certify that Council endorses the proposal shown on this plan.

Council Resolution:.....Of

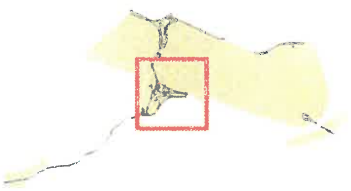
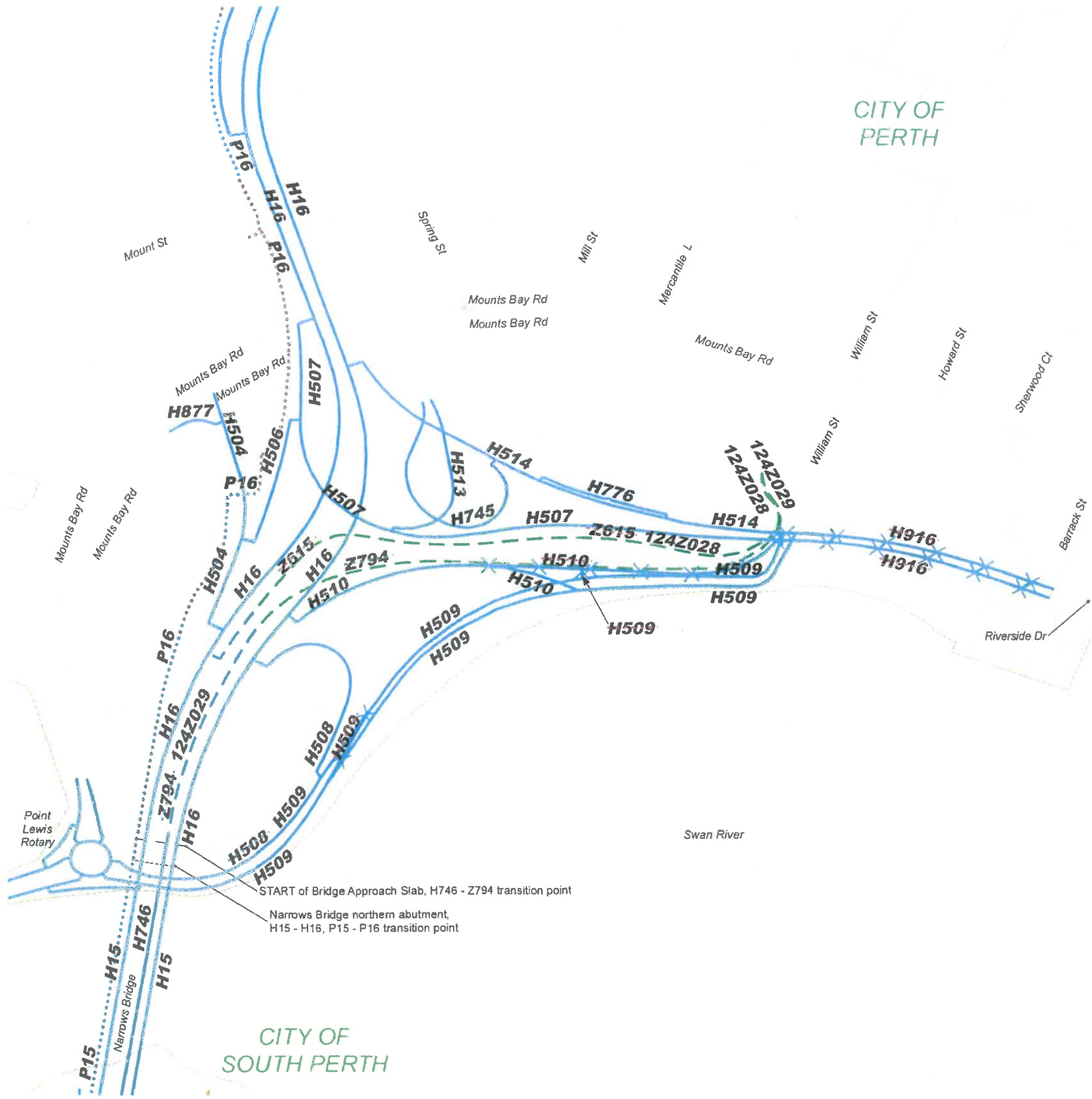
Council Meeting Held On:.....

Chief Executive Officer

Date:.....

Gazette:.....

Page No:.....



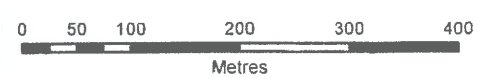
CITY OF PERTH

BASE MAP LEGEND

- Local Government Boundary
- Town Site Area And Boundary
- Cadastre
- Local Road

NOTE FOR DETAIL OF TOWNSITES - REFER TO TOWNSITE DRAWINGS

DATE OF ORIGINAL ISSUE 23 Dec 2009
DRAWING AMENDMENT DATE 14 Mar 2014



PROCLAMATION PLAN

Declared Roads
Narrows Interchange Proclamations
Deproclamations and Renumbering
CITY of PERTH - LG No. 124

MAIN ROADS WESTERN AUSTRALIA

| DWG TYPE | FILE No | DWG NUMBER |
|----------|---------|--------------|
| 20:00 | 11/7480 | 0921-0188-03 |

RECOMMENDED
APPROVED
EDPRS
22/12/14

Planning and Technical Services Directorate

SHEET 1 of 1



Enquiries: Ron Tolliday on 9323 4473
Our Refs: 11/7480 D14#692991
Your Ref:



ABN: 50 860 676 021

~~19 December 2014~~

15 JANUARY 2015

Mr G Stevenson
Chief Executive Officer
City of Perth
GPO Box C120
PERTH WA 6839

Dear Mr Stevenson

PROCLAMATION OF CHANGES TO ROADS WITHIN THE NARROWS INTERCHANGE

As part of the Elizabeth Quay development ramp H916 has been removed as a road from the network. Also a number of other ramps and PTA bus ramps have been realigned, renumbered or renamed.

Will you please seek formal Council agreement to proclamation drawing 0921-0188-03, which supersedes drawings 0921-0188-02 endorsed by Council at its June 2012 meeting.

Subject to receiving that agreement, will you please add Council's resolution number and date to the drawings, then sign and return one original to Main Roads, marked to the attention of Road Classification Manager, Ron Tolliday. The other original should be retained as Council's interim record, pending formal proclamation. Following proclamation, a copy of all the final drawings for the Narrows Interchange showing gazettal details will be sent to Council for its records.

If you require any further information regarding the proclamation action, please contact Ron Tolliday on 9323 4473. Any enquiries relating to management of the road and paths and delineation of responsibility between Main Roads, your Council and the Public Transport Authority, should be addressed to our Manager Metropolitan Road Program.

I await receipt of Council's response.

Yours sincerely

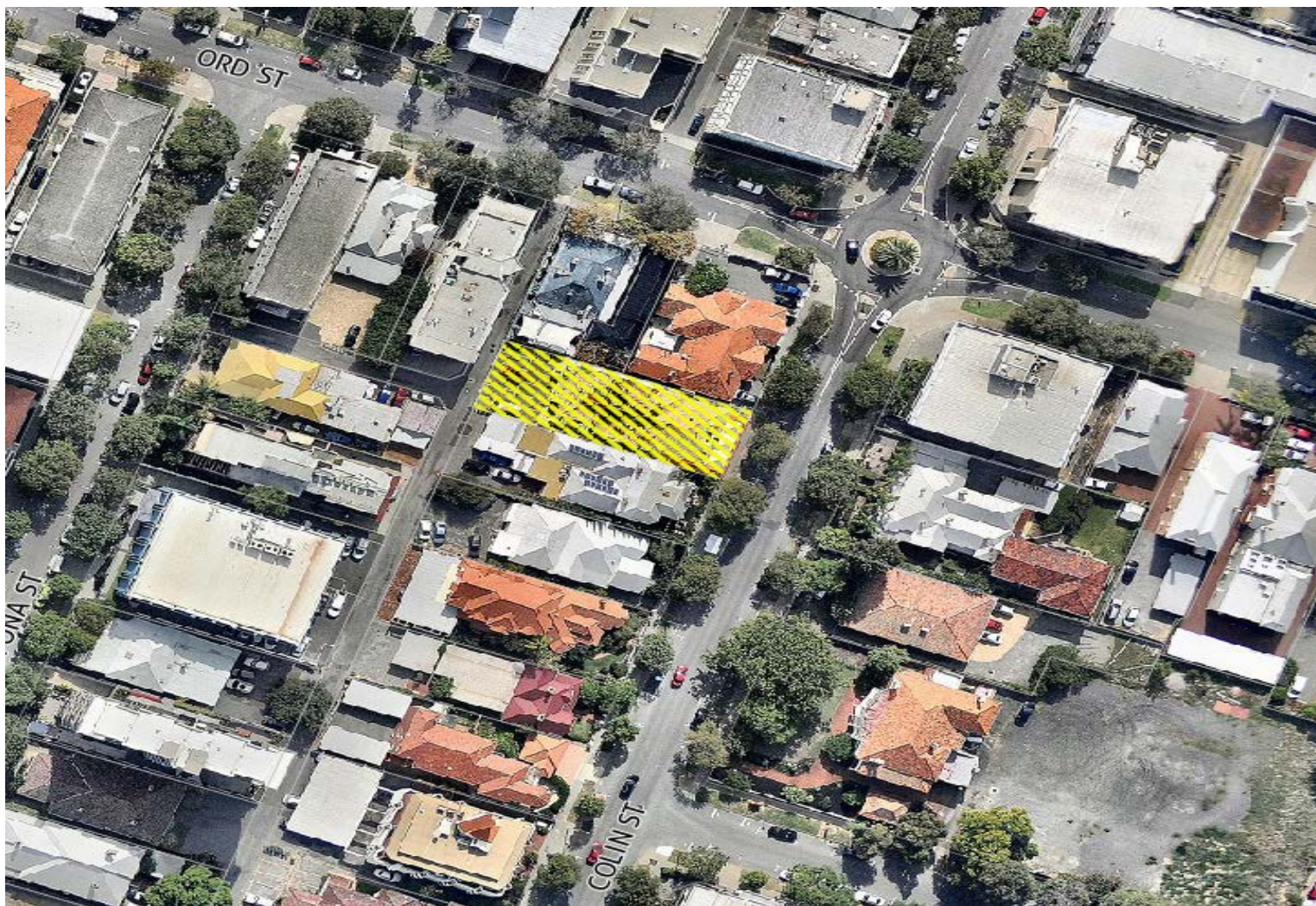
Doug Morgan
EXECUTIVE DIRECTOR
PLANNING AND TECHNICAL SERVICES

encl.



Australian Business
Excellence Awards
Bronze Award 2007

SCHEDULE 11



SCHEDULE 11

2015/5206; 21 - 23 COLIN STREET, WEST PERTH



2015/5206; 21 - 23 COLIN STREET, WEST PERTH